



MOSSDALE LANDING WEST

SPECIFIC PLAN

a community designed for the
City of Lathrop

12/16/2024

PAGE INTENTIONALLY BLANK

**MOSSDALE
LANDING
WEST**
SPECIFIC PLAN

a community designed for the
City of Lathrop

Prepared for: The City of Lathrop

Prepared by: Westwood

TABLE OF CONTENTS

EXECUTIVE SUMMARY	8
INTRODUCTION.....	10
1.0 Relationship to West Lathrop Specific Plan	10
1.1 Specific Plan Contents	10
1.2 Historical Context	12
THE DEVELOPMENT PLAN.....	20
2.0 Residential	21
2.1 Public Lands	23
2.4 Circulation.....	27
DEVELOPMENT STANDARDS.....	36
3.0 Low Density Residential	36
3.1 Architecture Standards	36
3.2 Landscape Architecture Standards	36
3.1 Architecture Standards	43
3.2 Landscape Architecture Standards	45
PUBLIC SERVICES	66
4.0 Fire Protection.....	66
4.2 Solid Waste & Recycling	67
4.3 Parks.....	68
4.4 Maintenance CFD	69
4.5 Schools.....	69
INFRASTRUCTURE	72
5.0 Introduction.....	72
5.1 Storm Drainage and Water Quality	72
5.2 Domestic Water	74
5.3 Wastewater Treatment Options	74
5.4 Recycled Water	75
5.5 Dry Utilities	77
5.6 Electricity and Natural Gas.....	77
5.8 Telecommunications	77
IMPLEMENTATION	80
6.0 Entitlements	80
6.1 Financing Mechanisms.....	80
6.2 Financial Plan	80
6.3 City Approval Process.....	81
6.4 Amendment Process	83
6.5 Differences Between the West Lathrop Specific Plan and the Mossdale Landing West Specific Plan	84

APPENDIX "A" ARCHITECTURAL STYLES	89
APPENDIX "B" PLANT LIST.....	94
APPENDIX "C" LATHROP MUNICIPAL CODES (LMC)	97
APPENDIX "D" ADOPTED GENERAL PLAN CONSISTENCY TABLE	98

LIST OF FIGURES & TABLES

FIGURE 1.1 PROJECT SITE MAP	11
FIGURE 1.2 REGIONAL MAP	13
FIGURE 1.3 PHOTO LOCATION MAP	14
FIGURE 1.4 EXISTING SITE PHOTOGRAPHS.....	15
TABLE 2.1 LOT SIZE AND DIMENSION	21
FIGURE 2.1 LOT SIZE MAP	22
FIGURE 2.2 NEIGHBORHOOD PARK SERVICE AREA MAP	24
FIGURE 2.3 PUBLIC OPEN SPACE MAP	25
TABLE 2.2 PARK ACREAGES.....	25
TABLE 2.3 QUIMBY ACT CALCULATION	26
TABLE 2.4 MOSSDALE LANDING WEST LAND USE SUMMARY	26
FIGURE 2.4 CIRCULATION MAP	28
FIGURE 2.5 BUS STOP CONCEPT	29
FIGURE 2.6 STREET SECTIONS	31
FIGURE 2.7 MULTI-USE TRAIL CONCEPT	33
FIGURE 2.8 BIKE/PEDESTRIAN MAP.....	34
FIGURE 3.1 ENHANCED ELEVATION LOTS	40
FIGURE 3.2 STREET TREE MASTER PLAN	44
FIGURE 3.3 COMMUNITY WALL CONCEPT	46
FIGURE 3.4 RESIDENTIAL LOT-STREET TREE DIAGRAM	47
FIGURE 3.5 GATEWAYS AND ENTRIES	49
FIGURE 3.6 NEIGHBORHOOD PARK CONCEPT PLAN	52
FIGURE 3.7 LINEAR PARK ADJACENT TO RIVER ISLANDS	53
PARKWAY CONCEPT PLAN.....	53
FIGURE 3.8 COMMUNITY PARK CONCEPT PLAN	54
FIGURE 3.9 OPEN SPACE CONCEPT PLAN: STREET W AND RIVER ISLANDS PARKWAY	55
FIGURE 3.10 MOSSDALE LANDING WEST GATEWAY CONCEPT PLAN.....	56
FIGURE 3.11 ROUNDABOUT CONCEPT PLAN.....	57
FIGURE 3.12 WALLS AND FENCES	59
TABLE 5.1 ESTIMATED WATER DEMAND FOR MOSSDALE LANDING WEST	74
TABLE 5.2 ESTIMATED WASTEWATER DEMAND FOR MOSSDALE LANDING WEST ...	75
TABLE 5.3 ESTIMATED RECYCLED WATER DEMAND FOR MOSSDALE LANDING WEST .	75
.....	75
FIGURE 5.1 MOSSDALE LANDING WEST UTILITIES MAP	76
TABLE 6.0 MOSSDALE LANDING WEST - STREET SECTIONS	85
TABLE 6.1 RIVER ISLANDS PARKWAY - STREET SECTION	85
TABLE 6.2 STREET W - STREET SECTION.....	85
TABLE 6.3 BARBARA TERRY BOULEVARD - STREET SECTION.....	85



INTRODUCTION

An aerial photograph showing a large area of agricultural land. The fields are arranged in a grid pattern, with various shades of brown, tan, and green. A winding river or canal flows through the landscape, curving around a large, light-colored, circular area that appears to be a reservoir or a large pond. The overall scene is a mix of cultivated and natural land.

EXECUTIVE SUMMARY

The Mossdale Landing West Specific Plan details planning, infrastructure and design for a future low density residential community within Mossdale Village. This document will lay out planning and design guidelines that set apart Mossdale Landing West as its own neighborhood within the larger master planned community.

Mossdale Landing West is a low-density residential community that offers a variety of single family homes (829 dwellings units) with lot sizes ranging between 3,360-5,000 square feet and public open spaces that encourages pedestrian oriented living. Of the 205.9-acre project, approximately 147 acres will be developed for single family homes and 13 acres devoted to public park space in the Mossdale Landing West community. This Specific Plan details out and builds upon the Mossdale Village plan and policies presented in the West Lathrop Specific Plan (WLSP).

The development of this new neighborhood will incorporate traditional elements found throughout Central Valley communities including a hierarchy of interconnected streets, the incorporation of assorted architectural styles, tree lined thoroughfares, an emphasis upon pedestrian scale and access with a nod to the agricultural traditions of the Valley. There will be a focus on local history and site context surrounding the City of Lathrop to better foster community inclusion and form neighborhood identity.

Development and design standards laid out in this document are intended to provide a framework for any improvements within Mossdale Landing West. The framework aims to reinforce the vision for the Mossdale Landing West community thus creating a holistic character and quality. Building parcels, architecture, utilities, and landscape architecture within the development will be discussed in detail.

Careful planning and implementation of the overall development is required to minimize the impact of construction to the surrounding areas, including the existing levee. All roads, site grading and utilities will be installed with this in mind.

Various financing options can be employed to implement the construction of the Mossdale Landing West project. All on-site improvements must be funded by the project, and the developer shall work with the City to create a plan to fund future maintenance and operations. These costs cannot be passed on to existing residents in the area.



INTRODUCTION

Mosssdale Landing West is a master planned community encompassing 205.9 acres primarily consisting of low density residential homes. This document seeks to create a consistent neighborhood concept within the Mosssdale Village development that integrates its rich local history and its unique geographic location while providing a livable pedestrian-oriented environment. This goal is consistent with the West Lathrop Specific Plan and envisions a total build out of 829 dwelling units with accessible open space for all.

Situated along the San Joaquin River, Mosssdale Landing West Specific Plan (WLSP) is located within the City of Lathrop just west of I-5. The San Joaquin River borders the north and western perimeter and offers opportunity for expansive views of the landscape and passive recreation along the levee. To the east of the development is Mosssdale Landing, a mixed use master planned community that will be integrated with Mosssdale Landing West.

The City of Lathrop adopted the West Lathrop Specific Plan in 1996 with the intention of developing and integrating land west of I-5 to increase community, economic and other benefits within the City. The West Lathrop Specific Plan outlined the development of two distinct areas, Mosssdale Village and River Islands. Mosssdale Village was planned to be a predominantly residential community with a variety of low to medium density type housing. Residents would enjoy a system of interconnected trails and pedestrian friendly streets. The levee bordering the perimeter of Mosssdale Landing West creates the opportunity for residents to live alongside and experience riverine habitats.

1.0 Relationship to West Lathrop Specific Plan

This Specific Plan serves to direct the course of the design and development for the Mosssdale Landing West community and emphasizes the creation of a pedestrian centered community through walkable streets, interconnected trails and accessible open space. The Specific Plan enforces the vision set forth in the WLSP, and includes elements of an Urban Design Concept.

The WLSP envisioned Mosssdale Village as a new residential community with access to public amenities such as a connected trail system, parks and a centrally located elementary school. The intent is to provide residents equitable access to amenities while creating distinctly unique neighborhoods. This Specific Plan follows that intent with the centrally located park, multi-use trail along the levee and mix of architecturally different houses and sizes.

Once approved this Specific Plan document serves as the framework for the City, developers, and builders to identify and enforce permitted land uses; architecture, landscape, and site planning standards; infrastructure improvements; and project implementation (per WLSP). This Specific Plan does not, in any way, alter, amend, or otherwise change the entitlement for Mosssdale Village and the West Lathrop Specific Plan. The intent is to have a stand-alone Specific Plan for the Mosssdale Landing West area.

1.1 Specific Plan Contents

The Mosssdale Landing West Specific plan is intended to provide a comprehensive view of all related land use and infrastructure policies that are necessary to implement the project. Per Sections 65451 of the California Government Code, a specific plan includes descriptive text and illustrations of land use and open space within the plan area. Specific plans lay out standards and criteria for proposed distribution, location, extent and intensity of public and private components, transportation, sewage, water, drainage, solid waste disposal, energy and other essential facilities required to support uses within the planning area. Implementation of these standards and criteria is crucial to ensure that the development can progress in a sustainable manner. Where there are discrepancies between the Specific Plan and Lathrop Municipal Code (LMC) the Specific Plan shall take precedence. In the event the Specific Plan fails to provide direction, the LMC should govern missing regulations and requirements.

FIGURE I.1 PROJECT SITE MAP



The following chapters break down each of these components and details out the standards and criteria specific to Mossdale Landing West.

Chapter 1-Introduction: Provides a brief overview of the project, specific plan process, document content, and project historical context.

Chapter 2-The Development Plan: Defines the proposed land uses and circulation within the development, and the standards for each, including an overview of proposed transportation systems such as roadways, bike paths and pedestrian walkways.

Chapter 3-Development Standards: This chapter utilizes descriptive text, diagrams and/or tables to convey to readers and the developer architectural and landscape guidelines to ensure a cohesive, high- quality built environment.

Chapter 4-Infrastructure: Summarizes utility related components such as sewer, water and drainage to support development.

Chapter 5-Implementation: This chapter describes the process and policies this development must adhere to for submittals and/or any amendments that may need to be made. In addition, a financing plan is included to ensure construction and design is cost effective and there is adequate funding to support the completion of the project.

Appendices: Two appendices are provided to supplement information provided in the plan.

1.2 Historical Context

Mosssdale Landing West is situated west of I-5 within the City of Lathrop and is bounded by the San Joaquin River on the west. The City of Lathrop is rich in culture and history having been a major transportation and agricultural center in the mid 1880's. Originally known as Wilson's Station, the town became known for the first ferry crossing along the San Joaquin which was later named "Moss Ferry" by Captain William Simms Moss, and the final link to completing the transcontinental railroad. The first train crossed in September 1869. The introduction of the railroad brought people into the area. A settlement started by Leland Stanford, began to grow around Wilson's Station. Stanford later renamed the settlement Lathrop after his brother-in-law Charles Lathrop¹.

The name Mosssdale comes from William S. Moss, an Ohio steamboat captain from the 1800's who owned most of the land in the area at the time. Originally from Virginia, Moss worked his way up to becoming a Captain. His success allowed him to invest in several businesses including railroads and agriculture. William Moss was captivated by the California Central Valley landscape during a trip out west. He purchased acres of land in San Joaquin County adjacent to the Moss' Ferry river crossing. This land is now Mosssdale Landing.²

In 1965, the state of California passed the Williamson Act also known as the California Land Conservation Act of 1965, as a way to protect agricultural land. The Williamson Act is intended to preserve agricultural lands through discouraging developing agricultural land to urban uses. Any development around agricultural land would be limited to compatible land uses. Enforcement of the Williamson Act is voluntary and would require private land owners to enter into contracts with local governments, agreeing to maintain agricultural land or open space as is. Landowners who enter into contracts would receive property tax assessments lower than normal.

Mosssdale Landing West planning area falls under the provisions of the Williamson Act and would require a cancellation of Williamson Act Contracts before development can occur.

The Mosssdale Landing West project proposes to update the land uses set forth in the WLSP. The land historically was used for agriculture. The WLSP indicated the development of marina housing and waterfront commercial in the area, but has been since amended to feature low density single-family homes and parks. This will increase the density of the area to better meet the demand for housing.

1 Leland and Jane Stanford. https://www.ci.lathrop.ca.us/sites/default/files/fileattachments/community/page/3541/updated_-_amasa_leland_stanford_park_-_final.docx.pdf. City of Lathrop

2 Captain William Simms Moss. https://www.ci.lathrop.ca.us/sites/default/files/fileattachments/community/page/3541/updated_-_william_s._moss_-_final.pdf. City of Lathrop



HISTORIC LATHROP PHOTOGRAPHS, CITY OF LATHROP



HISTORIC LATHROP PHOTOGRAPHS, CITY OF LATHROP

FIGURE I.2 REGIONAL MAP

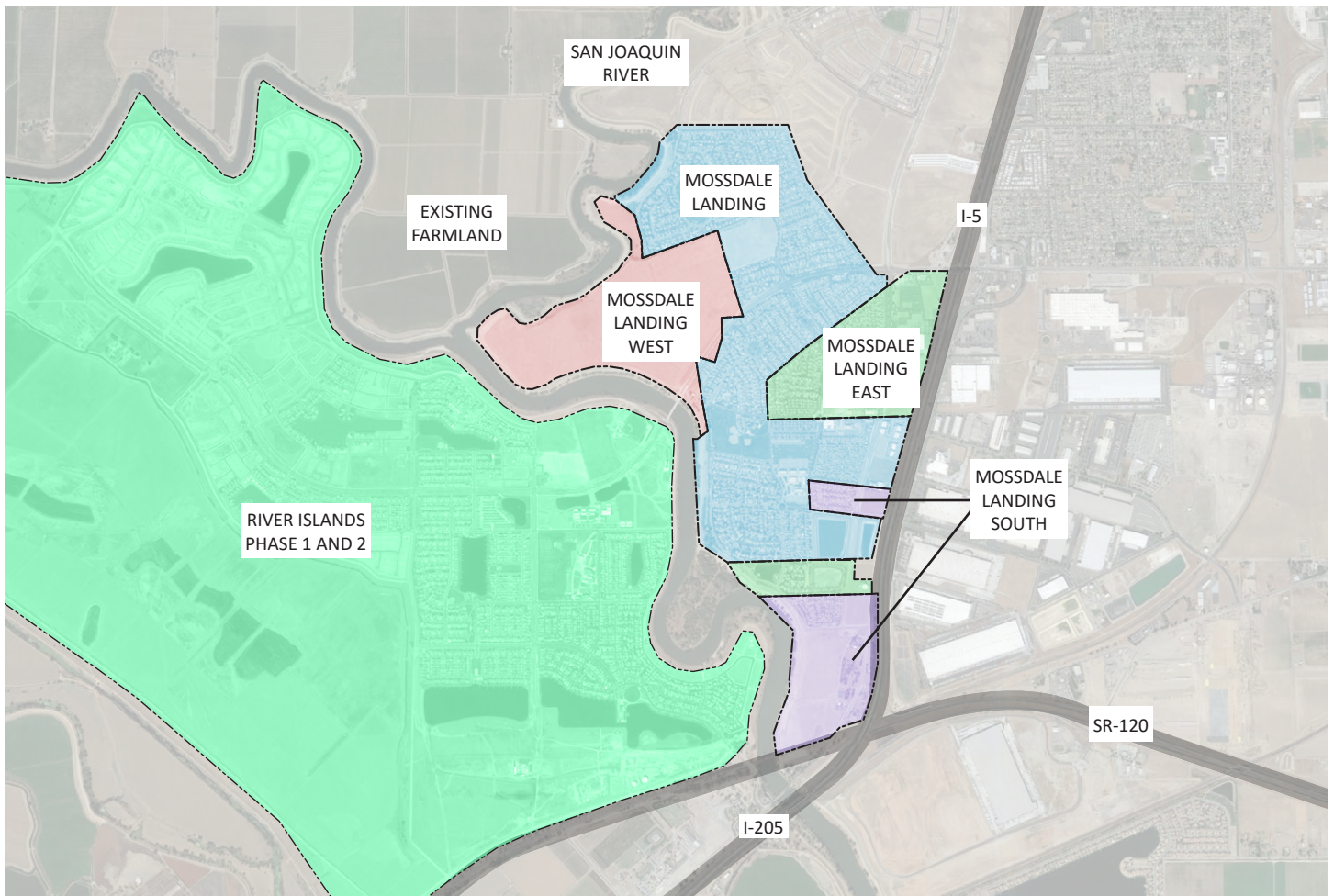


Figure VI-1 of the WLSP, shows the project site as part of the fourth and final stage of development in Mossdale Village. The concept behind this final stage of development was to create a “marina or river oriented” development connecting residents to the San Joaquin River. Phase 4 was to include a total of 482 low density housing units, 173 of which would be marina housing, with a population of approximately 1,562 people. A four-acre Village Center and four acres of waterfront commercial were initially planned for this phase of development along with parks, a 50-acre high school site which has since been installed north on Spartan Way and a road along the river’s edge. Due to hurdles during environmental permitting, the marina/river oriented concept was abandoned. Concerns regarding the structural integrity of the levee system running parallel to the river’s edge made any concept of developing a lake or marina type development infeasible. A more traditional layout and approach was adopted for the proposed project area.

Mossdale Landing West is a unique site within Mossdale Village. It only consists of residential, public uses, and one major roadway. Mossdale Landing West will provide a singular housing type: low-density, single family detached housing units, governed by the development standards under Low Density in the WLSP. WLSP defines Low Density as 3 – 9 dwelling units per net acre with maximum coverage of 50%. The number of dwelling units provided in the Mossdale Landing West is 829, which results in gross density of 5.65 units per acre.

In Mossdale Landing West, four lot sizes are proposed ranging from 3,360 square feet to 5,000 square feet with two different lot frontage widths and three different lot depths.

FIGURE I.3 PHOTO LOCATION MAP



FIGURE I.4 EXISTING SITE PHOTOGRAPHS



PHOTO 1: MOSSDALE LANDING WEST ALONG EASTERLY WALL



PHOTO 2: MOSSDALE LANDING WEST LOOKING TO CENTER OF THE SITE



PHOTO 3: LOOKING TOWARDS SUBDIVISION TO THE EAST



PHOTO 4: BARBARA TERRY BOULEVARD LOOKING EAST



PHOTO 5: BARBARA TERRY BOULEVARD LOOKING EAST



PHOTO 6 : INTERSECTION OF HOMESTEAD AVE AND UPPER PASTURE
LOOKING TOWARDS OPEN SPACE EAST OF MOSSDALE LANDING WEST
SITE



PHOTO 7: UPPER PASTURE ROAD



PHOTO 8: HOUSES ON UPPER PASTURE ROAD



PHOTO 9: AREA SOUTH OF RIVER ISLANDS PARKWAY AND NORTH OF TRESTLE POINT



PHOTO 10: AREA SOUTH OF RIVER ISLANDS PARKWAY AND NORTH OF TRSTLE POINT



PHOTO 11: PROPERTY SOUTH OF RIVER ISLANDS PARKWAY



PHOTO 12: EXISTING HOME SOUTH OF RIVER ISLANDS PARKWAY



PHOTO 13: EXISTING PROPERTY ON THE SOUTH SIDE OF RIVER ISLANDS PARKWAY



PHOTO 14: LOOKING NORTH TOWARDS RIVERS ISLANDS PARKWAY



PHOTO 15: LOOKING NORTH TOWARDS RIVER ISLANDS PARKWAY



PHOTO 14: RIVER ISLANDS PARKWAY -LOOKING WEST TOWARDS SAN JOAQUIN RIVER



PHOTO 16: RIVER ISLANDS PARKWAY-LOOKING TOWARDS EXSITING SUBDIVISION



PHOTO 17: RIVER ISLANDS PARKWAY LOOKING EAST TOWARDS MCKEE BOULEVARD

PAGE INTENTIONALLY BLANK



2

DEVELOPMENT PLAN

THE DEVELOPMENT PLAN

The Mossdale Landing West Specific Plan is based upon the Mossdale Village plan and policies presented in the West Lathrop Specific Plan (WLSP), which is consistent with the City of Lathrop's General Plan. The project provides the approximate acreages of the following land uses – 146.7 acres of Low-Density Residential, approximately 18.0 acres of Public designated uses that are made up of 4.8 acres of linear park, 6.2 acres of neighborhood community park, 2 acres of parkland dedication south of River Islands Parkway, 3.6 acres of other open space (including landscaped entries), and 1.4 acres of levee slope easement. There is also a remainder of 38.2 acres of undeveloped land. The plan provides a total of 829 dwelling units, which results in a density of 5.65 dwelling units / acre.



Bounded by the San Joaquin River with view accesses to nearby open farmlands, Mossdale Landing West is planned to provide traditional residential options for permanent Lathrop residents with scenic visual views to natural settings and unique opportunities for river-front activities. Mossdale Landing West is characterized by a broad open space corridor along the San Joaquin River, which surrounds the neighborhoods on the southern, western, and part of the northern side. This open corridor that runs along the River is part of the regional district-wide trail system. It provides a venue for day and night recreational uses. Sidewalks and trails supplement the neighborhood's circulation network to provide safe and pleasant pedestrian use and low-speed traffic experiences. A large community park is planned in the center of the western side of the neighborhood. Low density neighborhoods maintain the openness of Mossdale Landing West. Off-site vehicular traffic mainly stays on River Islands Parkway, making Mossdale Landing West a semi-enclosed community with a relatively private feel. With the focus on the pedestrian, Mossdale Landing West will be an enjoyable community for residents and visitors alike.



2.0 Residential

In Mossdale Landing West, four lot sizes are proposed ranging from 3,360 square feet to 5,000 square feet with three different lot frontage widths and four different lot lengths.

TABLE 2.1 LOT SIZE AND DIMENSION

LOT SIZE	LOT DIMENSION	# OF LOTS	DU/AC
3,360 SF	42' x 80'	54	12.96
3,570 SF	42' x 85'	50	12.20
3,375 SF	45' x 75'	142	12.90
4,000 SF	50' x 80'	144	10.89
5,000 SF	50' x 100'	439	8.71



FIGURE 2.1 LOT SIZE MAP



2.1 Public Lands

Public designated lands within Mossdale Landing West include a 30' wide linear waterfront park, a 6.2 acre neighborhood park, and other open space with a total acreage of 16.0.

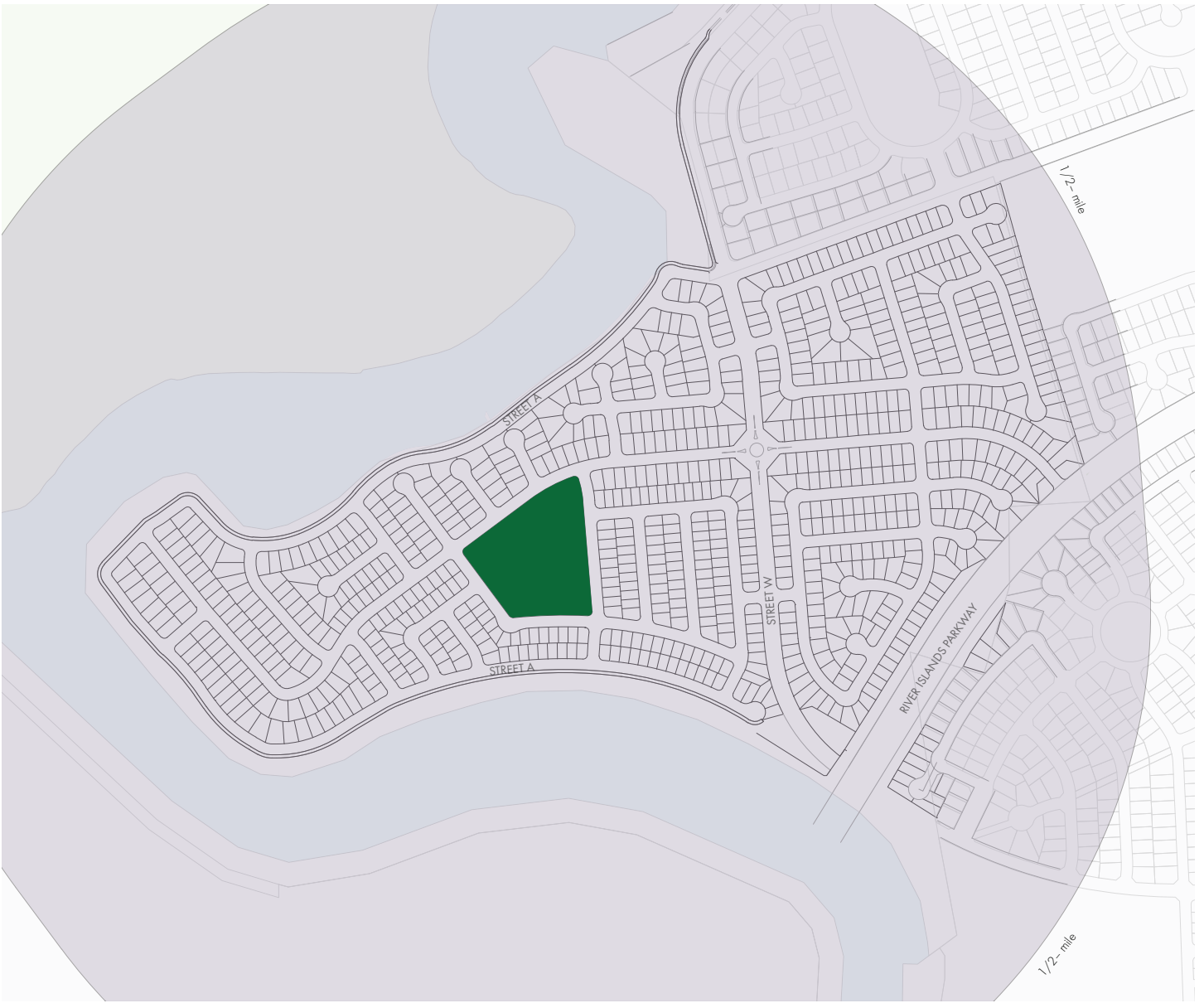
A 6.2-acre neighborhood-oriented park is located at the west-central portion of the property. This park is within 10 minutes of walking distance of all residences within the neighborhood. As the only large open gathering space within the neighborhood, this park is intended to be a local destination for various activities such as small events, parties, and general recreational uses for all age groups. Neighborhood parks differ from “Community” parks in that it serves the immediate area and generally have lower volume of visitors.



The WLSP designated a system of trails along Mossdale Village’s westerly San Joaquin River edge as a community resource to be shared and used by all Mossdale Village residents. Within the Mossdale Landing West boundary, this trail is an extension of the existing trail area from the Mossdale Landing development to the south and continues from the southwest corner of the site, runs along the San Joaquin River, and ends at the north end of the neighborhood, where it continues into Stanford Crossing Community. This linear park segment within Mossdale Landing West is approximately a mile and a half in length. It is planned to provide the most enjoyable outdoor waterfront experiences for all Mossdale Village residents for quiet and safe activities such as walking, jogging, and biking. Proper lighting design will be incorporated to allow for evening uses. Both parks within Mossdale Landing West are easily accessed from the surrounding neighborhoods.

There are approximately 5 additional acres of public land designation in Mossdale Landing West, consisting of parkland dedication space, landscape areas at roundabouts, remnant parcels, and various pedestrian connections between the neighborhoods. The landscaped areas at the roundabout at Street W and Street C create a strong sense of place in the neighborhood. The neighborhood entry at Street C and River Islands Parkway, a landscaped gateway, will be created for residents and visitors coming south bound from the River Islands Parkway.

FIGURE 2.2 NEIGHBORHOOD PARK SERVICE AREA MAP





-  1/2-Mile Buffer
-  Neighborhood Park

FIGURE 2.3 PUBLIC OPEN SPACE MAP



- 30' Wide Linear Park*
- Neighborhood Park
- Parkland Dedication
- Other Public Green Space

*See Figure 2.6 'Street A' Street Section and Figure 2.6 for section of multi-use trail concept along Linear Park.

TABLE 2.2 PARK ACREAGES

PARK	ACREAGES
Linear Park	Appro. 4.8 ac
Neighborhood Park	Appro. 6.2 ac
Parkland Dedication	Appro. 2 ac
Other Public Green Space	Appro. 3.6 ac
Total	Appro. 16.6 ac

Since the 1975 Quimby Act (California Government Code §66477) was passed, cities and counties in the state of California have had the power to establish land dedication ordinances and require that developers contribute in one of several ways to dedicating land for park improvements. Quimby Act standards include dedication of 3-5 acres per 1,000 residents in a given jurisdiction to ensure adequate park land is developed along with housing construction. While five acres does not constitute a maximum allowable park acreage, it does represent in the Quimby Act the maximum park acreage per 1,000 residents that may be required for developers. The City of Lathrop has further defined these acreages by requiring three (3) acres of Community Park per 1,000 residents and two(2) acres of Neighborhood Park per 1,000 residents. Per the City’s General Plan, minimum acreages apply to the sizes of individual community and neighborhood parks. Per Quimby Act calculation shown in Table 2.3, Mossdale Landing West has a deficient park acreage of 1.62 acres and does not meet the minimum park land requirement. The remainder 1.62 acres of park land shall be mitigated through in-lieu fees paid to the City by the developer

TABLE 2.3 QUIMBY ACT CALCULATION

	STANDARD	REQUIREMENT	MOSSDALE LANDING	DIFFERENCE
Dwelling Units			829	
Population	3.65 per DU		3,026	
Community Parks (C)	3 acres/1,000	9.07 acres	7.3 acres*	-1.77 acres
Neighborhood Park (N)	2 acres/1,000	6.05 acres	6.2 acres	+0.15 acres
Total C&N PARKS	5 acres/1,000	15.12acres	13.5 acres	-1.62 acres

*For Quimby Calculation, community park acreage was determined by adding dedicated community park land (Lot C), and the lineal foot of the multi-use trail at the linear park, multiplied by the width of the trail to determine linear park acreage.

2.3 Mossdale Landing West Development Concept

Planned primarily for residential uses, Mossdale Landing West incorporates parks as a major amenity space to enhance the quality of life of its residents and contribute to the entire Mossdale Village community. Although the proposed land uses in the Mossdale Landing West Specific Plan slightly differ from the WLSP, the proposed plan meets the intent of the WLSP. Below are land use summaries that provide an overview of land uses, gross acres, and gross density, of the proposed project.

TABLE 2.4 MOSSDALE LANDING WEST LAND USE SUMMARY

LAND USE DESIGNATION	ACRES	DWELLING UNITS	AVG. DENSITY (du/ ac.)
Low Density Residential (RL-MV)	146.7	829	5.65
Public (P-MV)	13.0	N/A	N/A
Public/Quasi Public (P/QP-MV)	3.6	N/A	N/A



With the other parts of Mossdale Village providing a wide range of housing opportunities and institutional uses, Mossdale Landing West focuses on low density residential and the creation and continuity of the linear park space along the San Joaquin River. This will add to the heritage and visual characteristics of the bigger Mossdale Village community. Figure 2.1 illustrates Mossdale Landing West’s proposed lot sizes.

2.4 Circulation

Street System

A hierarchy of arterial, collector, and local streets are proposed to provide connectivity to and through the community to create a safe and efficient circulation network. High-speed arterial streets link to local streets through collectors which transition motorists to safe speeds through the community. Mossdale Landing West will be accessed primarily by River Islands Parkway, which is conceived as a major arterial that goes through the entirety of Mossdale Village into River Islands located to the west. Street W will be a collector road, on the east side of River Islands Parkway and extends through Mossdale Landing West connecting with Barbara Terry Boulevard. Street W will provide secondary access to the neighborhood via Street C and continued connectivity to the rest of Mossdale Village.



Arterial streets are major thoroughfares that move large volumes of vehicles through a space. These streets connect people from major centers of an urban area. Collector streets help in transitioning vehicles from high speed and large scale arterials to small scale local streets. These streets typically have landscape parkway strips separating vehicle traffic from the pedestrian sidewalk. Canopy trees should be planted within the parkway strips at even and consistent intervals to help establish a sense of space and help reduce the radiated heat from adjacent streets. Local streets are designed to be pedestrian oriented and supplement the circulation system that accommodates only necessary vehicular trips for mainly residents. Local street widths strategically slow vehicular traffic to improve the pedestrian and bicyclist experience.

River Islands Parkway has a 156-foot street right-of-way, which consists of a 16-foot wide median, two 31-foot-wide roadways in each direction and a buffered 8-foot-wide sidewalk on each side. River Islands Parkway landscaping shall match the existing landscaping on the south side of the street. Street W is designed to be an 80-foot-wide street with 5-foot-wide buffered sidewalks on both sides. Barbara Terry Boulevard is adjacent to the north side of the project. It will be built out to its full right-of-way width of 90 feet with full right-of-way improvements. All other local streets are designated to be 62-feet wide including a 5-foot buffered sidewalk on each side. See Figure 2.6 for street section details.

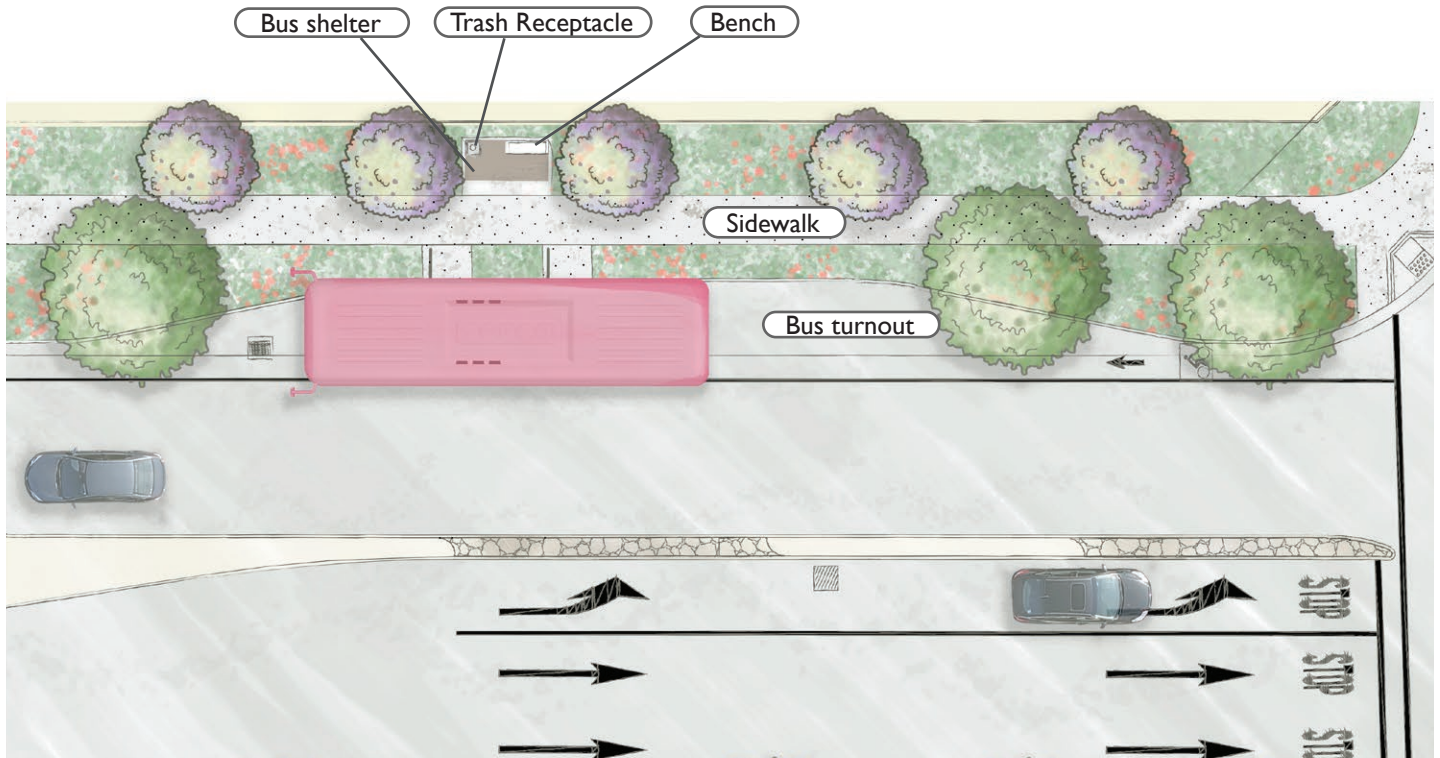
Public Transit

Proposed bus stop locations are shown in Figure 2.4. Final transit locations and bus stop design shall be coordinated with the San Joaquin Regional Transit District. Furnishings selected for bus stops shall match those approved by the City of Lathrop for Mossdale Landing West. Refer to the Street Furniture section in Chapter 3 for additional information.

FIGURE 2.4 CIRCULATION MAP



FIGURE 2.5 BUS STOP CONCEPT





Pedestrian System

Sidewalks are provided on both sides of the right-of-way throughout Mossdale Landing West to promote the walk-ability of the neighborhoods.

Trails and Bicycle Lanes

Trails and bicycle lanes provide residents an alternative to vehicular travel and promote a healthy lifestyle. The project proposes a network of bicycle lands and multi-use trails that link this community with Mossdale Landing. In-road bike lanes will be provided on River Islands Parkway and Street W. A pedestrian sidewalk adjacent to a Class I bike path in the linear park will be provided on Street A. As part of the greater bike and trail network of Mossdale Landing, the pedestrian and bike systems within this Specific Plan not only adds value to the neighborhoods but also to the regional wide trail facilities by providing multi-use opportunities and improved connectivity.

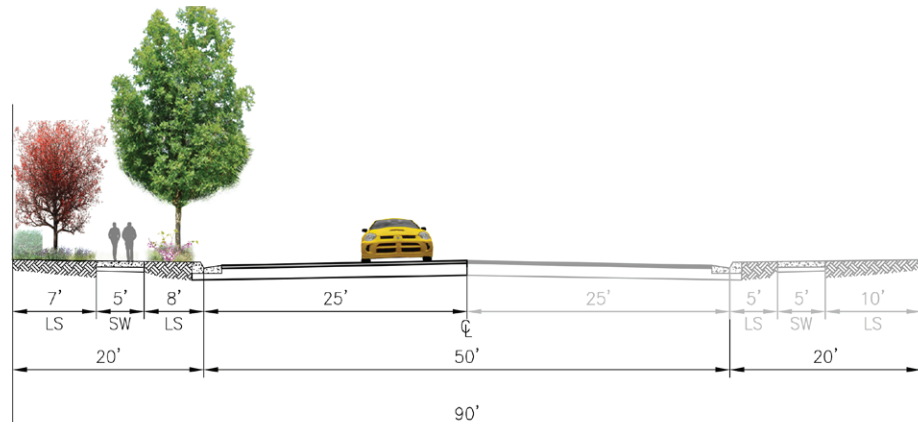
Multi-Use Trails

Multi-use trails are widened walkways that allowed for shared use between pedestrians and bicyclist. These trails are typically 15' wide with a section that is striped to indicate a Class I bicycle path. Refer to Street Sections Figure 2.6 and Figure 2.7 for more details. Trails are not intended to provide a connection or access to the levee patrol road.

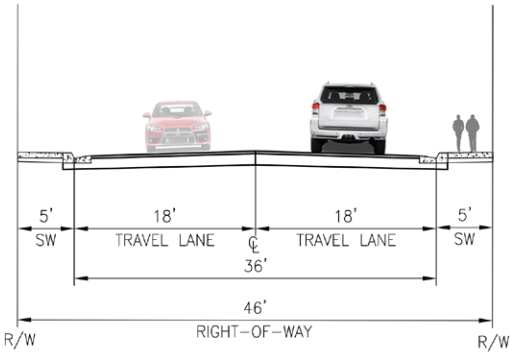
Bicycle Lanes

Class II bicycle lanes are designated striped lanes for bikes located adjacent to vehicular lanes. Bicycle lanes are located along Street W and River Island's Parkway, shall be 5' wide and made of asphalt. Refer to Street Sections Figure 2.6 for details.

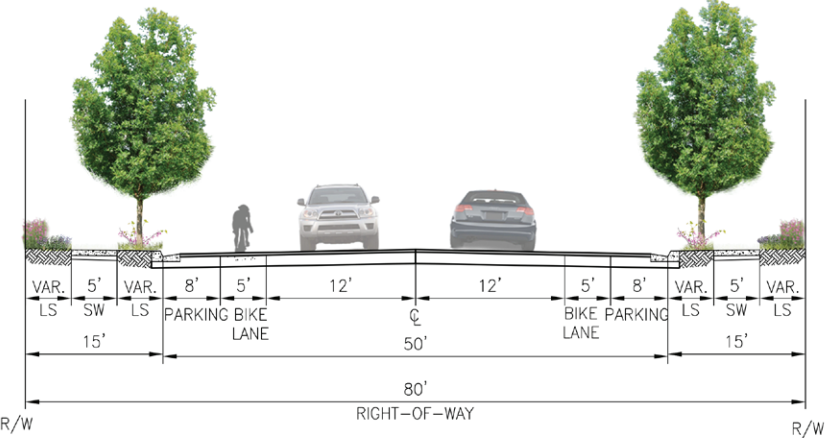
FIGURE 2.6 STREET SECTIONS



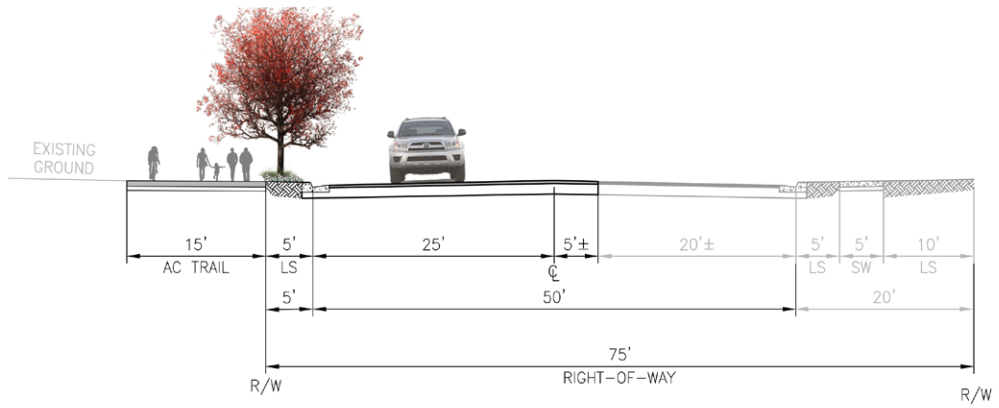
BARBARA TERRY BOULEVARD



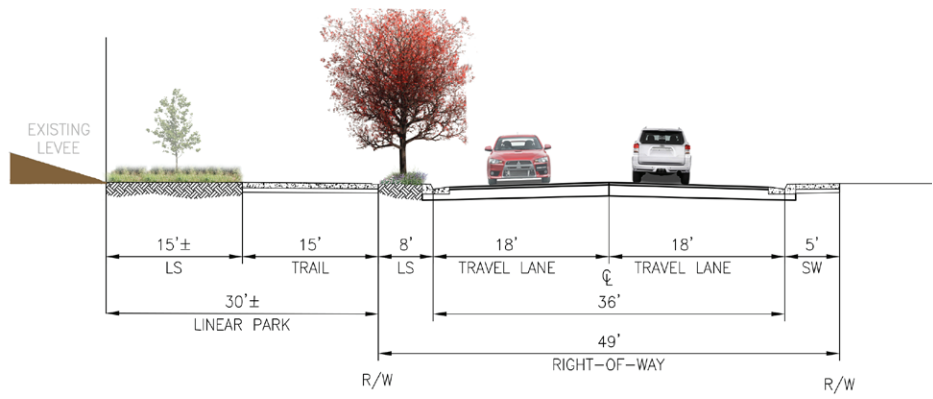
INTERIOR STREETS



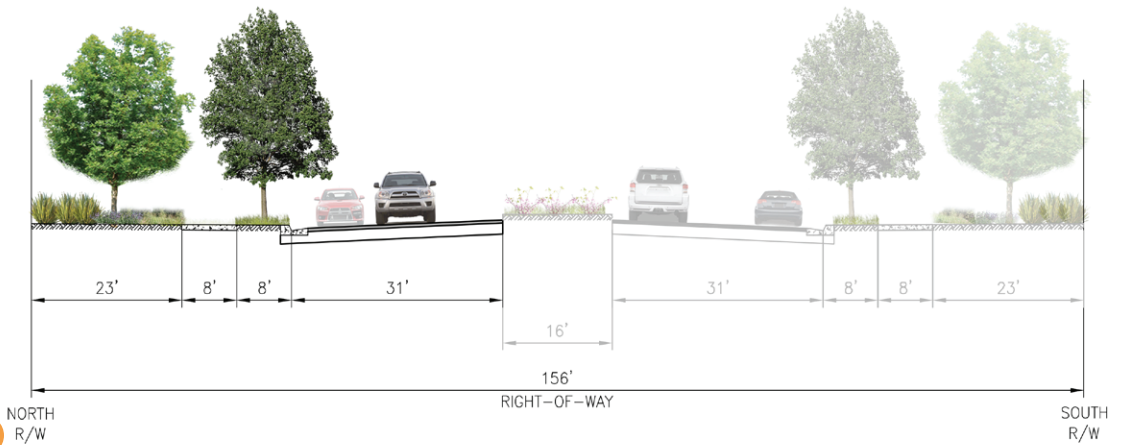
STREET W



Spartan Way

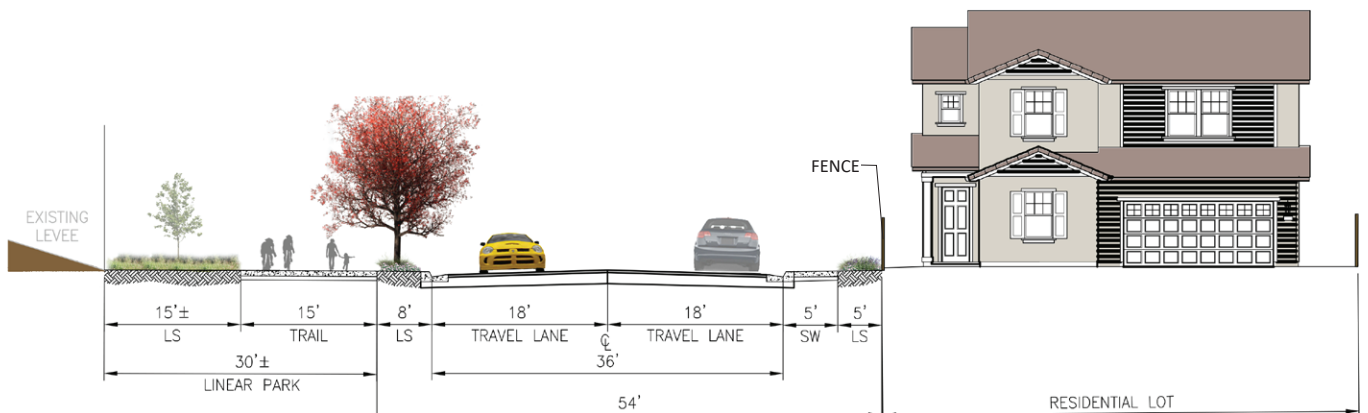
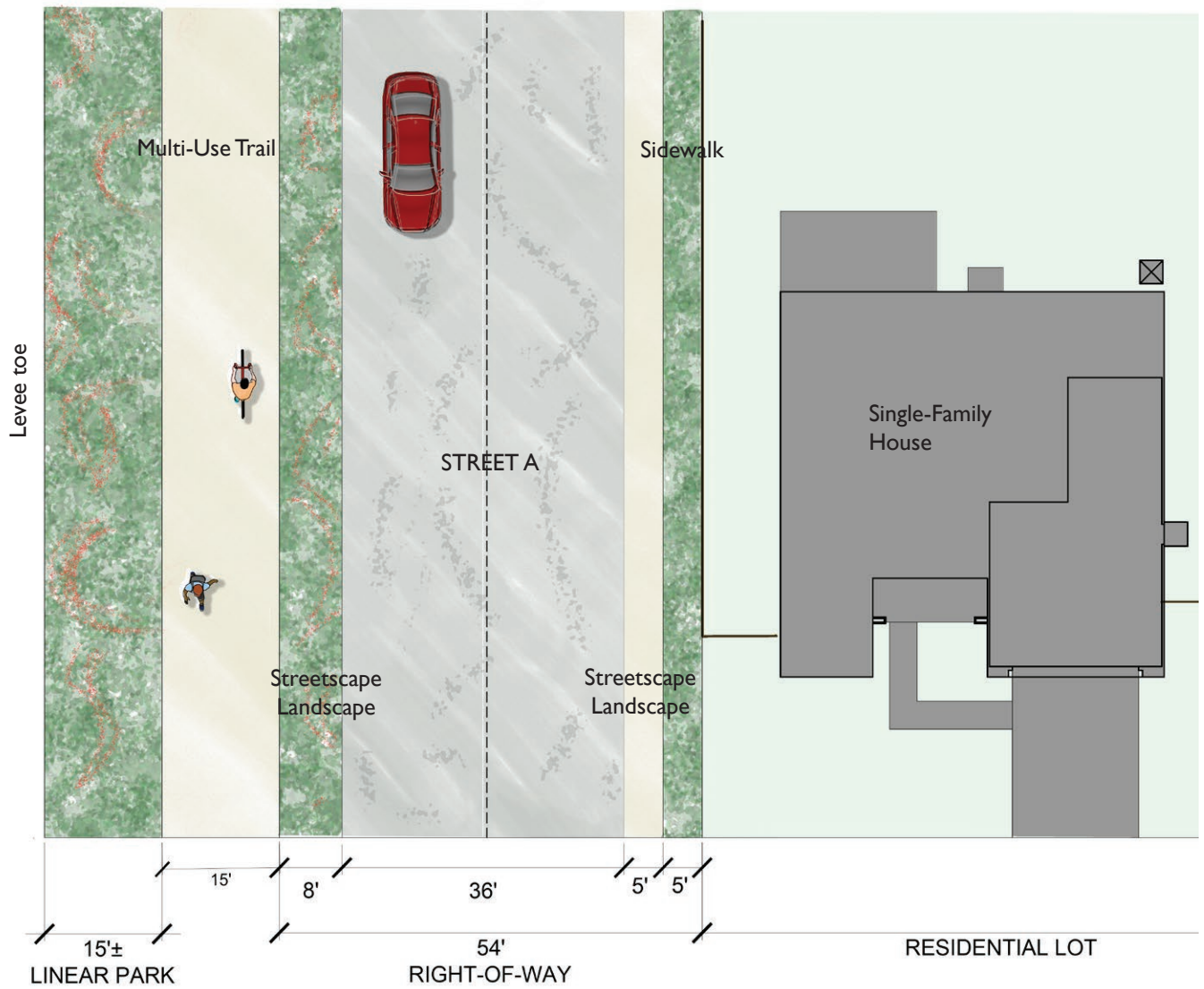


STREET A (49' ROW)



RIVER ISLANDS PARKWAY

FIGURE 2.7 MULTI-USE TRAIL CONCEPT



STREET A (54' ROW)

FIGURE 2.8 BIKE/PEDESTRIAN MAP





3

DEVELOPMENT STANDARDS

DEVELOPMENT STANDARDS

Mossdale Landing West is primarily a low density residential neighborhood with public open spaces woven throughout the district. Development standards contained in this document are intended to ensure consistency of major public elements within the development, and unify the adjoining Mossdale Landing developments.

The following standards are explained through descriptive text, diagrams, or tables to convey the intent of the development and are intended to provide conceptual guidelines for builders.

3.0 Low Density Residential

Single Family Homes

The following section details development standards for low-density housing (single family detached housing units). Low density housing accounts for 100% of Mossdale Landing West’s residential product type. Lots sizes vary between 3,360 square feet to 5,000 square feet. The standards outlined in the section is intended to create distinct and varied lotting patterns that create neighborhood character, provide privacy and minimize monotony along the street scene.

Massing and Detail

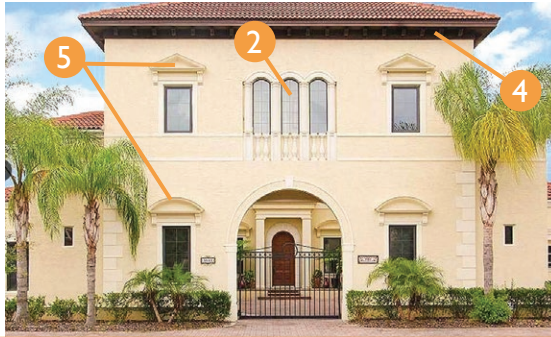
Variety in street scene along residential streets is important for establishing neighborhood character. Setback requirements should be utilized to avoid linear repetition. Consider siting buildings at different setbacks, varying garage and building entry orientation from adjacent buildings and providing architectural elements such as porches, windows or columns. Driveway lengths should ensure adequate space for vehicles to avoid blocking sidewalks. Streets that are dominated by garage doors are strongly discouraged.

Front and side building elevations should be varied to create visual interest. This can be done through setbacks and architectural elements to vary depth, height and visual aesthetic of the building. Units abutting River Islands Parkway should be visible from the street and have enhanced elevations. See examples on the following page. At least one floor plan for each home builder shall be single story to create varied vertical interest. Buildings adjacent to public trails, parks or open space should have fronts oriented towards the street or amenity. Two-story buildings facing overlooking off-street trails, major streets or open space are required to have enhanced elevations that may include windows or doors. While there may be different elements used throughout the neighborhood, attention to scale, detail and proportion to one another should be considered to maintain neighborhood cohesiveness.



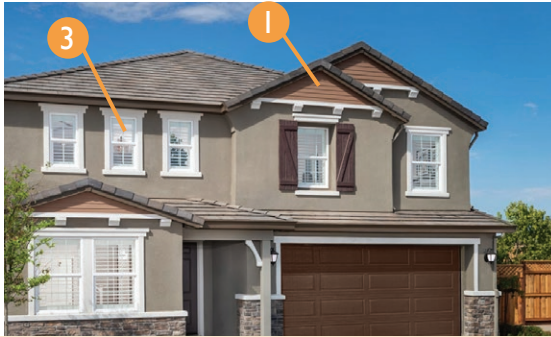
Examples of Enhanced Elevations

1. Tower and/or cupola
2. Tall/narrow windows
3. Cast iron decoration
4. Overhanging/deep eaves
5. Elaborate window crowns (arched or with brackets/pediments)



Italian

1. Decorative gable-end detailing
2. Use of stone & brick
3. Windows grouped in three or more
4. Panel shutter design
5. Off-white and light tones with contrasting trims/accents



English Country

1. Clapboard, board/ batten, wooden shingle or siding as primary building material
2. Decorative knee brace
3. Heavy square or tapered columns on brick or stone piers
4. Wide entry porch
5. Panel door



Craftsman/Bungalow

1. Arched entry
2. Use window shutters
3. Windows grids and recessed massing
4. Tiled roofs
5. Accent flare wall massing



Spanish

Examples of Single Story Elements

Italian

Masonry/Stone Veneer
Recessed Porch
Deep Eaves
Shutters



English Country

Timber Entry Framing
Multi-pane Windows
Wood Siding



Craftsman/Bungalow

Barge Rafters
Open Front Porch
Exposed Beams
Tapered columns



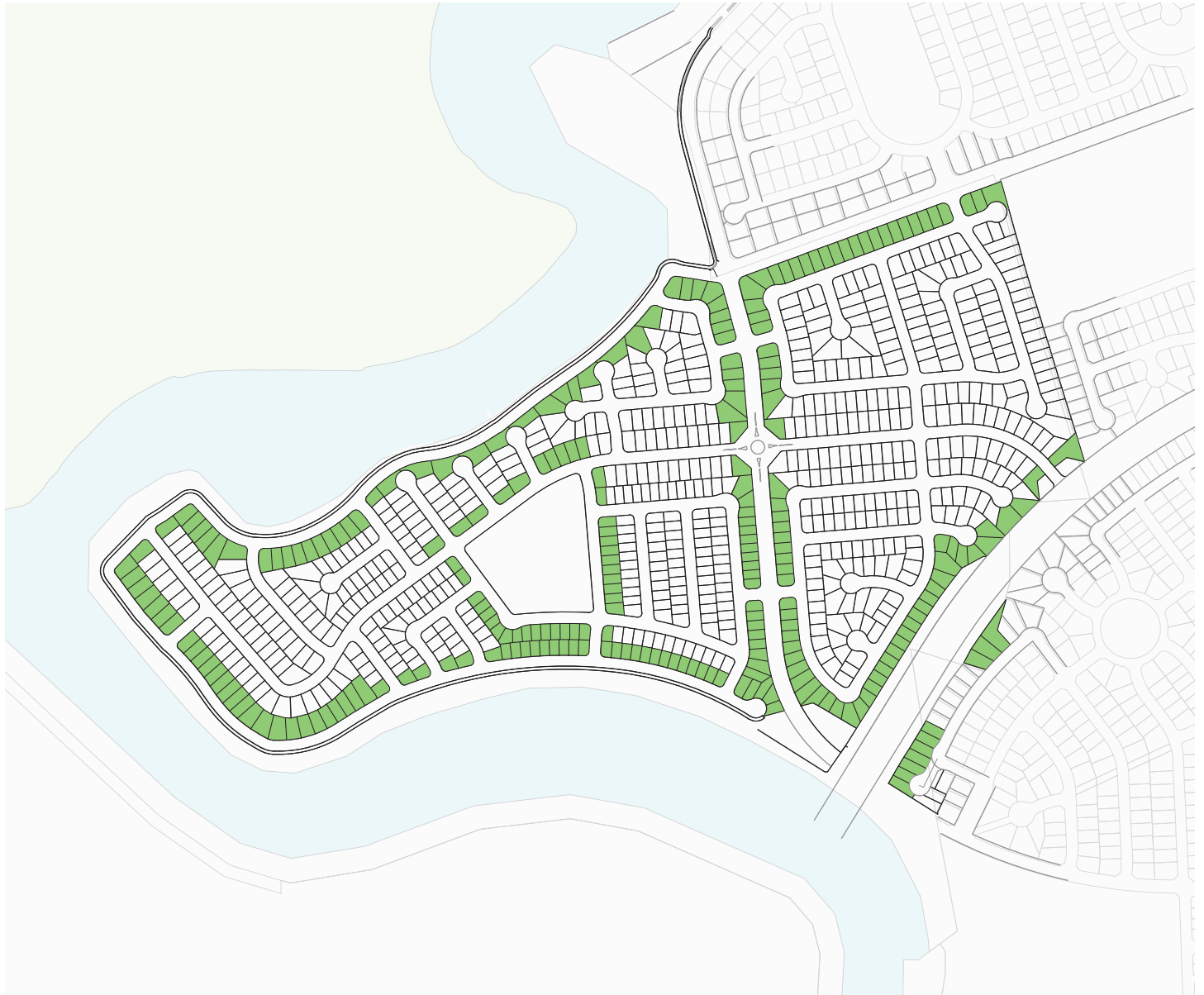
Spanish

Multi-pane Windows
Ornate/Detailed Front Entry
Carved Stone



	DEVELOPMENT STANDARD	GUIDELINES
MASSING AND DETAIL	<ul style="list-style-type: none"> • A minimum of one single-story floor plan shall be required per home builder. • At least half of homes shall have significant single-story elements on front elevations. See single story element examples. • Half of all floor plans shall have recessed garages at least five feet behind front elevation or patio. • Units that back or side onto River Islands Parkway, trails and major streets shall have enhanced elevations where visible. Refer to Figure 3.1. 	<ul style="list-style-type: none"> • Porches or port cocheres may be approved as a single story element if incorporated into the architecture. • Provide a variety of setbacks and massing along the streets. • Pay attention to execution of scale and detail of architectural styles. • Maintain consistent scale of elements throughout the design. • Design individual elements in proportion to each other.
FENCING (MAJOR ROADWAYS)	<ul style="list-style-type: none"> • Solid fencing shall be provided along major roadways, such as Street W, River Islands Parkway, and Barbara Terry Drive. See Walls and Fences Figure 3.11. 	
FENCING (RESIDENTIAL LIMITS)	<ul style="list-style-type: none"> • All fences shall comply with LMC 17.92.070 N with the exception of fence height. • Fences on the front, side and rear yard shall not exceed 6' in height and be made of wood materials. 	
PORCHES AND DECKS	<ul style="list-style-type: none"> • A minimum of two floor plans must incorporate porches. • Porches must be a minimum of 50% of façade and minimum five feet deep. • A minimum of two floor plans must have a porch option that can be converted to a wraparound corner treatment. 	<ul style="list-style-type: none"> • Porches and decks shall reflect the appropriate scale and detail of the architectural style. • Where feasible, wrap around porches at corner lots should wrap at a minimum of five feet onto the side façade, depending on architecture.
PORTE COCHERES	<ul style="list-style-type: none"> • Porte cocheres must be less than twelve feet in height. • A three feet minimum side yard setback is required. 	<ul style="list-style-type: none"> • Incorporating porte cocheres are encouraged to create a visual filter for garages
GARAGE AND DRIVEWAYS	<ul style="list-style-type: none"> • For recessed garages, a minimum five foot setback is required between garage front elevation and living area or porch elevation. • No one garage location type may exceed half of a neighborhood's unit design. See garage location type examples. • Front facing three car garages are not permitted. • Garage doors shall be roll-up and door shall be recessed at minimum twelve inches behind the garage wall plane. 	<ul style="list-style-type: none"> • Avoid making the garage/driveway the primary feature of a home by varying location and door pattern. • Reduce the overall visual mass of the garage through the use of design treatments (windows, door pattern or color). • Alternate driveways as much as possible along a street. • Vary driveway widths as appropriate to the plan. • Hollywood driveways (driveways that permit low groundcovers planted in the center of driveway) are strongly encouraged along long driveways.

FIGURE 3.1 ENHANCED ELEVATION LOTS



■ Units with Enhanced Elevations

Examples of Garage Locations



Mid Recessed Garage



Deep Recessed Garage



Forward Garage



Forward Swing-in Garage

<p>GARAGE LOCATION TYPES (See examples on page 40)</p>	<ul style="list-style-type: none"> • Forward Swing-In Garage - May be located at the front, side or rear of plan to reduce visual mass. Refer to LMC 17.38.060I for setback requirements. • Forward Garage - Garage is located in front of living area or front of home. If this garage placement is utilized, add extra architectural treatments such as low wall with gates, porte cochere, decorative garden fence or additional landscaping. • Deep Recessed Garage - Garage located min. 20' behind the living area. May be detached. • Mid Recessed Garage - Garage located 10' behind the living area. May be detached. 	
<p>ROOFS</p>	<ul style="list-style-type: none"> • Mechanical equipment is not permitted on roofs. • Roof penetrations for vents shall be on the rear side of roof ridges where possible. Paint vents to match the roof color. • Roof material shall be tile. 	<ul style="list-style-type: none"> • Place satellite dishes away from the street.

<p>WINDOWS AND DOORS</p>	<ul style="list-style-type: none"> • All windows shall have surround treatment. • Openings of elevations shall be articulated with appropriate head jamb, and sill details. • Window grids, where used shall be used on street facing elevations. • Shutters shall be traditional in design and size in proportion to the architectural style. 	<ul style="list-style-type: none"> • Jamb trims may be added where appropriate. • Windows may be in various shapes and sizes. Double entry doors with or without side panels may be provided as long as they are appropriate to the building style.
<p>FRONT YARD SETBACK (from front property line)</p>	<ul style="list-style-type: none"> • 10' to living area or architectural projection • 20' to front-on garage(face of garage door)from public street. • 10' to swing-in garage 	
<p>REAR YARD SETBACK (from rear property line)</p>	<ul style="list-style-type: none"> • 10' minimum for single story units • For two story homes, including decks, balconies and other platforms with floor level over 6' in height, increase the rear yard by 10' for each additional story 	
<p>SIDE YARD SETBACK (from side property line)</p>	<ul style="list-style-type: none"> • 5' minimum • For corner lots at street side yard: 10' minimum 	
<p>ACCESSORY STRUCTURES</p>	<ul style="list-style-type: none"> • Comply with LMC 17.32.050. 	
<p>SWIMMING POOL</p>	<ul style="list-style-type: none"> • Comply with LMC 17.32.050L 	
<p>ACCESSORY DWELLING UNITS AND</p>	<ul style="list-style-type: none"> • Comply with LMC 17.80 	
<p>SIGNS</p>	<ul style="list-style-type: none"> • Permanent outdoor advertising structures and signs are not permitted. • Comply with LMC 17.84 and signage section on page 61-62. 	
<p>ENCROACHMENTS</p>	<ul style="list-style-type: none"> • All non-fire rated encroachments must be at least 3' from property lines and not infringe into public service or utility easements. • Encroachments may not exceed 25% of the length of the façade. <p>Example of Encroachments:</p> <ul style="list-style-type: none"> -Fireplaces; -Porches; -Log storage; -Entertainment niches; -Balconies (on front and rear facades only); -Bay windows; -Window seats; -Second floor overhands on front and rear only; -Decks 	



Parks

Mosssdale Landing West plans to have a centrally located neighborhood park and a linear park that circles the district’s western perimeter. These public open spaces are strategically located to be easily accessible by walking or biking to promote a healthy lifestyle. Landscaping and amenities of these public areas should remain consistent with the overall neighborhood character. Where possible, large shade trees should be placed in a manner to reflect traditional agricultural uses; these may include place large canopy trees arching over a street or pathway, in rows or in a pattern similar to orchards or groves. Plant palette shall relate to adjacent land use. Plant palette along the linear trail may vary and include species to mimic the riparian habitat along the San Joaquin. These species should be utilized along the edges closest to the levee.

Public/Quasi Public

Open space areas are intended for passive recreation, access to natural features, detention basins, and other storm water and water quality features, and trails. These spaces may include walking trails, seating, fitness equipment, and open turf for passive recreation.



3.1 Architecture Standards

This portion of the document provides direction and details into the overall building aesthetic that is desired for this community. Building styles should have consistent design features to create coherent neighborhood character. Architectural styles chosen for the low density units have been selected based on coherency, visual appeal and individuality. The following styles have been chosen for the low density units: Spanish, Craftsman, Italianate, and Cottage. These architectural styles are intended to be prescriptive and allow for interpretation to create uniquely distinct street scenes. Refer to Appendix A for more information.

FIGURE 3.2 STREET TREE MASTER PLAN



STREET	BOTANICAL NAME	COMMON NAME
Street W	Ginkgo Biloba 'Autumn Gold'	Autumn Gold Maidenhair Tree
Street A	Quercus lobata	Valley Oak
Street C	Ulmus parvifolia 'Drake'	Drake Chinese Elm
Street Q/ Barbara Terry Boulevard	Platanus acerfolia 'Bloodgood'	London Plane Tree
Residential N/S	Acer rubrum 'Redpointe'	Redpointe Maple
	Pistacia chinensis 'Keith Davey'	Keith Davey Chinese Pistache
Residential E/W	Koelreuteria paniculata	Golden Rain Tree
	Zelkova serrata 'Green Vase'	Sawleaf Zelkova
River Islands Parkway	Quercus coccinea	Scarlet Oak (See appendix B for complete list.)
ACCENT TREE LIST	Cercis sp. Chilopsis linearis Chitalpa tashkentensis Lagerstroemia indica Malus x 'Prairifire'	Redbud Desert Willow Chitalpa Crape Myrtle Prairiefire Crabapple



3.2 Landscape Architecture Standards

Streetscape

Landscaping along streets create pedestrian friendly environments through softening the edge between vehicular and pedestrian travel. Street trees often define street character and assist in establishing street hierarchy through size of tree canopy, foliage color and tree height. Proper selections of tree species along streets help identify and differentiate different communities, spaces and use. Streetscape landscaping should match scale and function of the street. Heavily traveled and regionally oriented streets should receive larger massing and heavier landscaping, while low trafficked streets should have landscaping scaled to support lower volumes of traffic. Tree species and character should reflect the local context.

Local neighborhood streets are intended to support lower volumes of traffic and encourage pedestrian and bicycle use. Neighborhoods should receive one to two varieties of street tree species to establish and maintain neighborhood identity.

River Islands Parkway

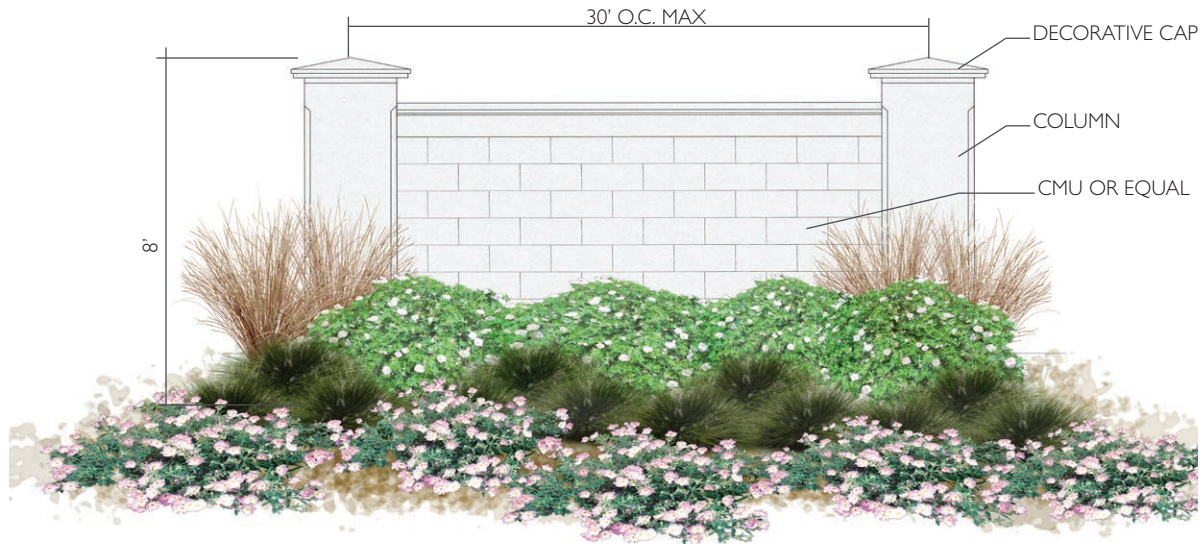
River Islands Parkway is a major east-west arterial that runs along the south side of the development. This major thoroughfare connects Mossdale Village and River Islands development. It serves as a key vehicular and pedestrian connector, as such, landscaping along the street should reflect and indicate its importance.

Large structural trees of the same species to match existing species in developed sections of River Islands Parkway. They shall be placed in a linear fashion on both sides of the street to frame the wide vehicular lanes. Trees within the median can be of the same species with a mix of flowering or accent trees. Spacing between trees should remain consistent and span the length of the street within this development. Landscaping should be integrated with existing developed sections to seamlessly integrate this project phase with the rest.

Understory shrubs and groundcover should be used to maintain a consistent landscape theme along the street. Large massing of plantings is encouraged over small groupings to distinguish River Islands Parkway as a major arterial. Plantings along the median and street perimeter should vary between flowering accent shrubs and grasses that are low maintenance and adapted to local climate. Perimeter boundary plantings should receive larger structured shrubs that take on their own natural shape without the need for pruning to maintain size and shape. The overall intent is to mimic fodder crop industry and grazing lands that are typical of the area. Rolling berms are not permitted within the right of way.

A community wall will separate any residential units that back onto River Islands Parkway. The community wall should extend the length of the street, be a minimum of six feet tall and match the existing sections that have been built. See Walls and Fences section this Chapter, for more information. A mix of shrubs, ground cover or vines shall be planted adjacent to the wall to soften and buffer the wall with the walkway. Reference Figure 3.12 for location of Community Wall.

FIGURE 3.3 COMMUNITY WALL CONCEPT



Residential Streets

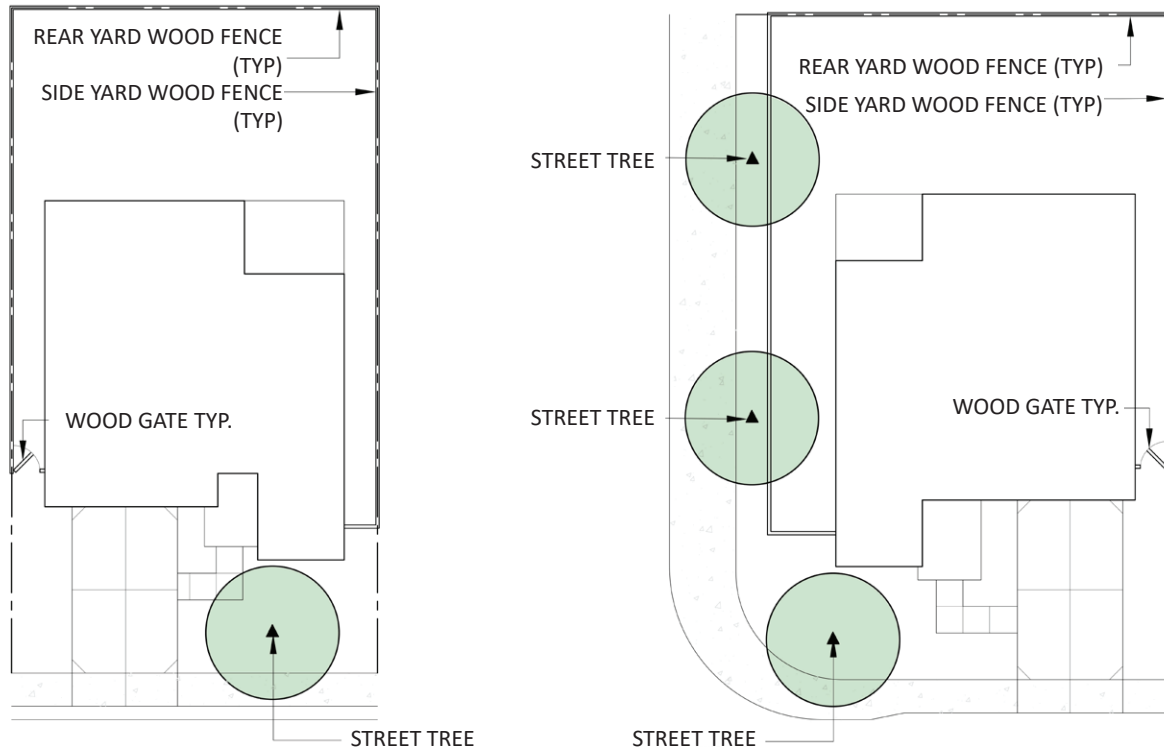
Residential streets are narrow two lane roadways that typically front residential homes or neighborhood parks. Due to the adjacent uses, these streets are pedestrian oriented and should be landscaped to match the surrounding scale and character. Pedestrian walkways shall be 5' wide. Large street trees shall be planted at 5' behind the sidewalk wherever possible, to provide shade and reduce the urban heat island. Corner lot conditions may require street trees to be planted at 2.5' behind the sidewalk. Street tree species should be different per neighborhood to establish unique neighborhood identity, buffer against potential tree die out due to disease or insects and support diversity within the urban forest. Street trees shall be planted in equal spacing with one tree in the front yard per interior lot and three per corner lot. See Figure 3.4 Residential Lot Street Tree Diagram. Similarly, one or two groundcover species shall be selected to complement street trees. See Street Tree Master Plan Figure 3.2 for suggested street tree and groundcover species. The property owner is responsible for the maintenance of street trees within property limits.

Street W

Collector streets facilitate efficient travel within the development and transitions higher speed traffic from larger arterials to smaller scale residential streets.

Street W is a neighborhood collector that runs north-south within Mossdale Landing West. Street W shall be characterized by a row of single species located within the interior parkways strips adjacent to the street. These trees should create visual and vertical interest. A mix of accent/flowering trees, tall and medium shrubs shall line the parkway strip located between community wall and the sidewalk. A 6' tall community wall shall separate any lots backing onto the collector. Both sides of the street shall provide a 5' wide meandering sidewalk for pedestrian travel. Parkway strips vary in width but shall be planted with a combination of shrubs and groundcovers to provide a buffer between the sidewalk and vehicular traffic. Plantings shall be grouped into larger masses and be consistent along entire length of the street. Refer to Appendix B for recommended plant species and sizes.

FIGURE 3.4 RESIDENTIAL LOT-STREET TREE DIAGRAM



Gateways and Entries

Gateways and neighborhood entries are located at important points of entry and is designed to establish a sense of arrival into a community. These focal points reinforce a community's identity and set a tone of the overall character. Features of these gateways and entries should reflect the scale or importance accordingly. Utilize a different plant materials, forms and shapes, materials and finishes to enhance and build distinctive character. Mossdale Landing West shall have two types of entrance features, major gateways and neighborhood entries. Refer to Figure 3.5 for gateway and entry locations.

Major Gateway

The intersection of River Islands Parkway and Street C is an important gateway for the community. This intersection signifies the main entrance and exit of the Mossdale Landing West community, and connects to a major arterial street, River Islands Parkway. Thus, the design and scale of this gateway should reflect the size and importance of its location.

The use of monuments, signage or gateway columns should be considered to highlight the entrance. Signage here should indicate Mossdale Landing West. External lighting may be used to further emphasize signage. Any monument, signage or gateway elements should utilize materials that reflect those historically used within the Central Valley. Enhanced paving and/or colored paving at crosswalks of entries along River Islands Parkway may be used to help reduce traffic speeds at the intersection.

The use of large trees with unique canopies or foliage is highly recommended to establish the area as a major point of interest. In addition, the use of flowering trees placed in a row to mimic orchards will help add interest. A landscape buffer consisting of large and medium shrubs shall be utilized to soften the community wall along the backside. It would be best to maintain symmetrical spacing for shrubs in this area.



Major Gateway Example

Neighborhood Entries

Neighborhood entries are smaller scale gateways that delineate entrances into residential neighborhoods. These entries act as important points of access and should be indicated through visual enhancements such as identification signage with neighborhood logo, lighting, enhanced hardscape or different landscape palette. In addition, neighborhood entries begin to display distinct neighborhood characteristics. This may include the use of accent or flowering trees and layering of different plants. Ensure neighborhood entry design utilizes a layered approach to understory planting to soften the edges along community walls. Neighborhood entry locations shall be determined during neighborhood lotting/ subdivision layout.

Signage for neighborhood entries may be utilized to indicate specific neighborhoods. Signs should be clear and simple and match the scale of the entry. Signage at all neighborhood entries should be uniform in style, color and materials to maintain a consistent community identity throughout the project. Entry columns may be utilized, but should be placed centrally either in the median or at the intersection corners while maintaining clear visibility for safety. If placed at intersection corners, columns should be paired across the right of way.

Enhanced pavement at crosswalks and/or along the entry shall be utilized to emphasize entry. The materials, colors, and finish should be similar or match those used on entry columns.

FIGURE 3.5 GATEWAYS AND ENTRIES





Parks and Open Space

Neighborhood Park

Mossdale Landing West will have one neighborhood park of approximately 6.22 acres. See Figure 3.6 “Neighborhood Park Concept” for conceptual design. This design is not final and will require further design development, undergo a review, and approval process with the Parks and Recreation Commission. This park is centrally located within the project and is intended to provide residents convenient access to public open space. This neighborhood park would contain amenities for passive recreation and a few programmed elements. Amenities may include and are not limited to basketball court, open field for free soccer play, tennis court, playground, picnic tables, shade structures, walking trails, etc. It is recommended to irrigate landscape areas within the park with reclaimed water. The use of reclaimed water will require City approval.

Design of the neighborhood park shall incorporate shade trees where possible to provide relief from the summer heat, typical of the local climate. Trees should be planted to emulate the characteristics of fruit or nut orchards. Take into consideration of views from residential yards, spacing for ball fields and courts. The use of climate adapted plants should be prioritized for ease of maintenance and longevity of plants. Site furnishings at the neighborhood park shall be consistent with the rest of Mossdale Landing West site furnishings. These designs are not final and will require further design development, undergo a review and approval process with the Parks and Recreation Commission.

Linear Park

The linear park sits at the toe of the levee slope edge and runs parallel with the San Joaquin River. The system is intended to provide residents a natural looking greenbelt with a multi-use trail for walking, biking, and jogging. Any amenities shall be restricted to flat areas and setback 10 feet beyond the toe of the levee slope to the adjacent street. Additional amenities such as benches, bike stations, picnic tables, adult exercise equipment stationed throughout the linear park is recommended to enhance user experience. Trees within the linear park shall be loosely and naturalistically planted to reflect the natural characteristic of riparian habitats. Informal planting of shrubs, grasses and groundcovers will further enhance the desired aesthetic. Structures are not permitted within 60’ of the levee toe. See Figure 2.7 for multi-use trail concept and Figure 2.6 for Linear Park cross section. These designs are not final and will require further design development, undergo a review and approval process with the Parks and Recreation Commission.

Open Space

Open space along the levee edge create a buffer between the San Joaquin River and Mossdale Landing West. Landscaping along the levee slope is not within the scope of this development. Any existing vegetation along the slope shall remain. Any changes to the levee slope area are subject to coordination and approval with Reclamation District 17, a District of the State Reclamation District who controls the levee system. In addition to the levee edge, pockets of open space are dispersed within the community to allow access to passive recreation and natural resources.

Roundabouts

Roundabouts have been placed within the project to facilitate efficient vehicular movement. Roundabouts shall be highlighted through focal elements such as a monument, accent trees, or trees that create visual interest. Roundabouts should be landscaped with shrubs and ground cover to delineate extent of roundabout. See Figure 3.11 for conceptual roundabout design.

Pedestrian Connections

Pockets of open spaces connecting neighborhoods to streets and parks allow for easy pedestrian and bicycle access. These areas shall be landscaped and provided a paved walkway connection.



FIGURE 3.6 NEIGHBORHOOD PARK CONCEPT PLAN (SUBJECT TO CHANGE)



FIGURE 3.7 LINEAR PARK ADJACENT TO RIVER ISLANDS PARKWAY CONCEPT PLAN (SUBJECT TO CHANGE)



FIGURE 3.8 COMMUNITY PARK CONCEPT PLAN (SUBJECT TO CHANGE)



FIGURE 3.9 OPEN SPACE CONCEPT PLAN: STREET W AND RIVER ISLANDS PARKWAY (SUBJECT TO CHANGE)



FIGURE 3.10 MOSSDALE LANDING WEST GATEWAY CONCEPT PLAN

(SUBJECT TO CHANGE)



FIGURE 3.11 ROUNDBABOUT CONCEPT PLAN (SUBJECT TO CHANGE)



Walls and Fencing

A hierarchy of walls and fencings help create usable spaces and are key to maintaining continuity and identity throughout the project. The following section details character, function, materials and permitted fencing and wall types for different spaces.

Community Walls

Community walls shall frame major streets and neighborhood entries. See Walls and Fences Figure 3.12. Walls shall be at most 6 feet tall except for River Islands Parkway where wall shall be 8 feet tall and match existing walls along the street. Where required, masonry sound walls will be deferred to noise studies. The community walls shall reinforce the traditional theme found throughout other Mossdale communities.

Placement of the community walls shall be at the right of way/ property line, private property side of the property line, or on public utility easement boundary. Columns shall be equally spaced at no more than 30' on center and protrude at least 6" from the wall on the public face. Community walls shall consist of concrete masonry units or similar. Detailed columns shall be incorporated at significant landmark locations directional changes or at wall end locations. Community wall panels shall receive a continuous cap. All columns shall have a separate decorative cap from the wall panels.

Neighborhood Lot Fencing

Neighborhood fencing is utilized within the residential areas to delineate residential private spaces. Fencing fronting any residential street or lot shall receive a decorative wood fence. Any fencing shared between lots shall be located on the rear and side property lines of the home lot, except at entries where community wall is specified between units. All neighborhood wood fences shall be 6 feet high, and uniform throughout the project unless otherwise noted.

Community Fencing

Community fencing is utilized within the residential areas along roadways to delineate residential private spaces. This fencing type fronting any residential street shall be topped with a decorative lattice treatment for enhancing the aesthetic. All community fences are to be made of wood and shall be 6 feet high, and uniform throughout the project unless otherwise noted.



COMMUNITY MASONRY WALL

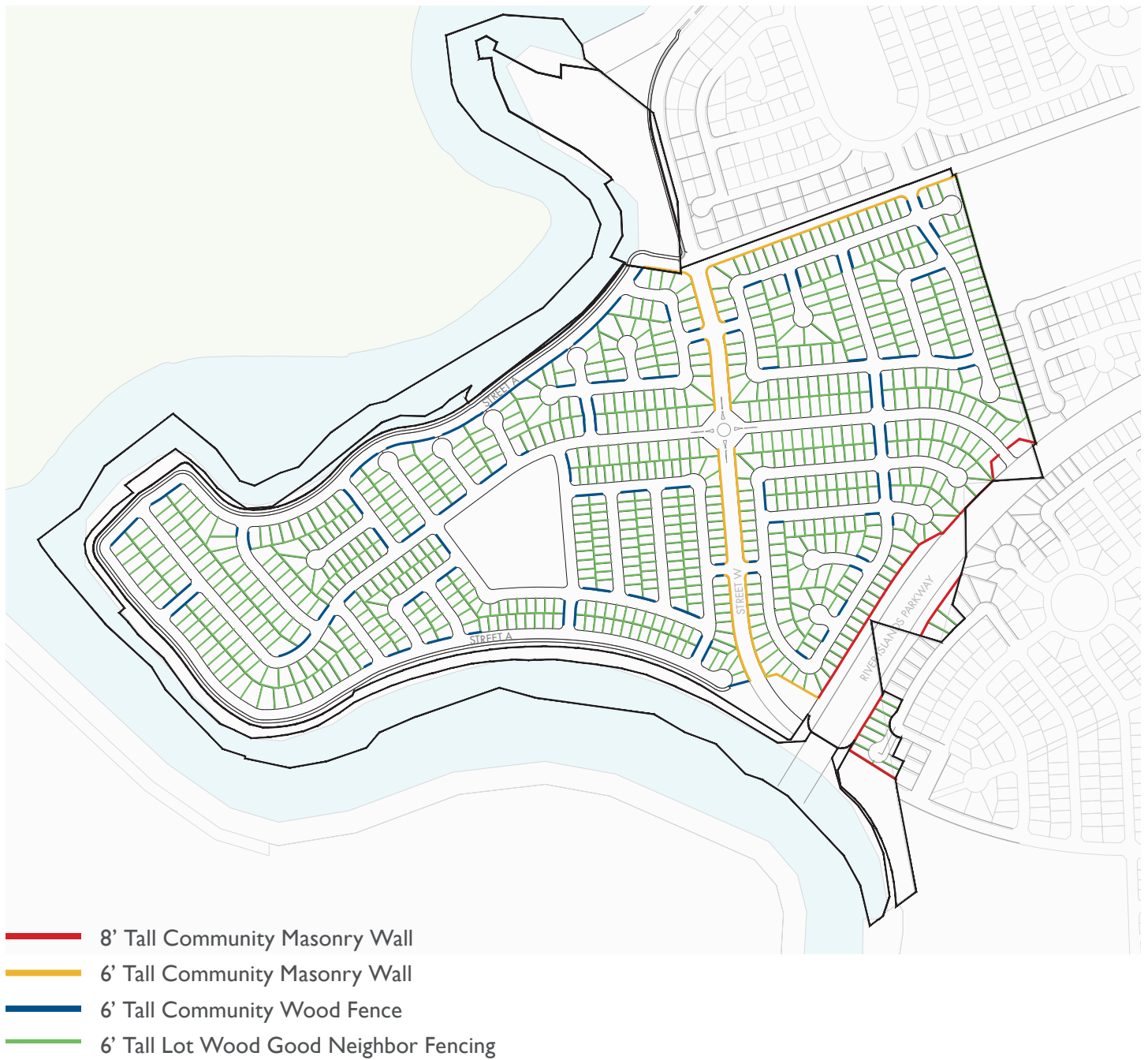


NEIGHBORHOOD LOTWOOD FENCING



COMMUNITY WOOD FENCING

FIGURE 3.12 WALLS AND FENCES



Note: Fencing subject to confirmation of a Noise Study.

Site Furnishings

Site furnishings shall be made up of consistent materials, colors and themes that promote community sense of place and identifiable neighborhood character.

Lighting

Lighting fixtures shall be designed to reflect the appropriate scale, type and illumination of the street or area is located. Lighting fixtures chosen shall reinforce the overall community theme. Park and open spaces may receive low level lighting such as bollard lights or pedestrian scale lighting.

Collector and residential streets may receive lower pedestrian level lighting. Pole heights along collector streets should be higher than those pole heights of residential streets. Fixture types, height and locations shall be consistent throughout the project. The lighting fixture standard for collector and residential streets shall be ornamental acorn-fixture lights unless an appropriate alternative is otherwise approved by the City.

All cast iron and steel light pole parts shall be factory finish painted "black green (RAL 6012)" unless an appropriate alternative is otherwise approved by the City. Color specified metals shall be powder-coated or anodized.

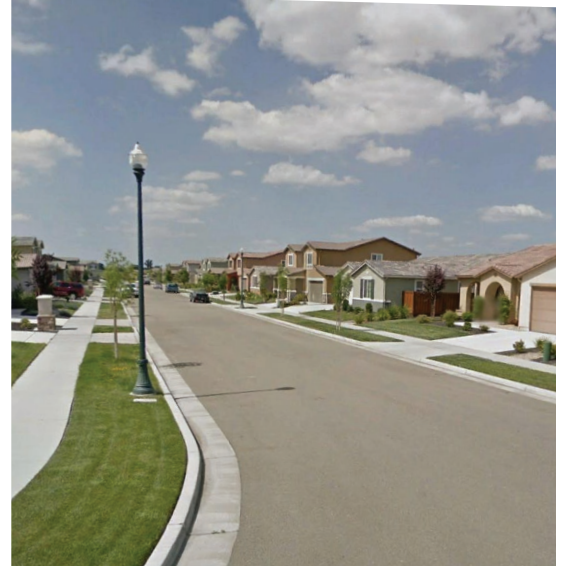
Shielding devices are required on light fixtures to prevent disruption to residential units. Lighting shall match City Department of Public Works standards and specifications as provided by the manufacturer, and meet City, PG&E and State of California standards for illuminations and safety unless an appropriate alternative is otherwise approved by the City.

Street Furniture

In order to maintain consistency throughout Mossdale Village, all street furniture shall match those adopted by the city of Lathrop for the Mossdale Landing project. Color of street furniture shall be "black-green (RAL 6012)" unless an appropriate alternative is otherwise approved by the City. Similar to lighting fixtures, all other color specified metals shall be powder-coated or anodized. Street furniture consists of benches, trash receptacles, and bus shelters.

Mailboxes

Mailboxes shall be from Florence Mail Boxes Florence vital 1570 series CBU in dark bronze or a City approved equal. Mail boxes may range from 8,12 or 16 compartments or model number 1570-8V2XX, 1570-12V2XX, 1570-16V2XX respectively. Mailbox locations shall be centrally located and placed in a logical manner to avoid blocking driveway access, views from windows or impeded vehicular or pedestrian traffic. As such, mailboxes shall be placed at side yard property lines wherever possible. Mailboxes are required to be installed behind the sidewalk with a minimum of 6" clearance between the face of the mail box and edge of sidewalk. Mailboxes shall be handicap accessible and approved by the United States Post Office.





Planting

Plant species and planting themes have been selected to mimic characteristics and habitat of delta waterways and agricultural landscapes. It is highly recommended to utilize plant material that is drought tolerant, durable and long-lived. Species should be well adapted to the climatic conditions and soil types of the project. Large naturally shaped flowering shrub species should be selected and placed in appropriate areas to imitate traditional hedgerows found in agricultural settings. Incorporate flowering species into planting design to create year round interest.

Avoid tree species with invasive characteristics. Some examples include species with surface roots that can spread and create tripping hazards, species that produce large wind-blown seed varieties, or varieties that are extremely allergenic or have toxic properties if digested by humans or animals.

When selecting plant species, consider species with that would minimize maintenance challenges. Fruiting tree varieties can cause problematic issues due to fruit litter attracting rodents and other pests or be a difficult to maintain. Utilize evergreen shrubs where appropriate for screening of fences or utility structures.

A mix of deciduous and evergreen tree varieties shall be utilized to create interest throughout the seasons. Select tree varieties that imitate the rural Central Valley aesthetic. Traditional "lawn" species is highly discouraged and should be limited to parks and public open spaces for recreational use. Use deep rooting species that use less water when "lawn" species are used. Groundcovers with interesting leaf structure or flowers should provide year round cover.

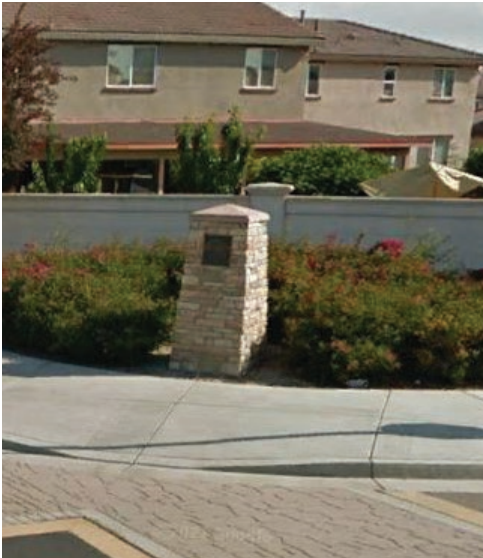
Plant materials will be selected from Appendix B: Plant Selection Guide. Landscape plans shall be prepared by a landscape architect licensed to practice in the State of California.



Irrigation

Irrigation of medians, public open space and parks within Mossdale Landing West shall comply with City of Lathrop Municipal Code Chapter 17.92-Landscape and Screen Standards and the California Model Water Efficient Landscape Ordinance (AB325), whichever is more stringent.

Irrigation design shall comprise of water saving techniques, be laid out to efficiently and uniformly distribute water to all plant material and installed to accommodate water demands by plant type. The use of low volume broadcast spray heads, drip and bubbler types is recommended throughout the project. Different hydrozones shall be placed on separate valves. Trees should be designed and placed on separate irrigation zones to promote long term health of trees.



EXAMPLE NEIGHBORHOOD ENTRY SIGNAGE



EXAMPLE PARK SIGNAGE

Signage

A successful signage system not only contributes to an efficient traffic circulation of vehicles, but assists in reinforcing neighborhood character and identity. Signage within the project shall be consistent and located based on signage hierarchy to facilitate proper directional instruction. The color, materials and design should be durable and vandal resistant.

Consider utilizing the area’s historical context as a basis for establishing street and park names as a unifying feature. Design standards in this section shall take precedent over those found in the City’s Zoning Ordinance.

Street/Vehicular Signs

Street signs contain street names and orient vehicular travel. These signs should follow a hierarchy in size that is appropriate for the accompanying street. For example, major arterial and collector streets should have larger signs for legibility. Mossdale Landing West shall use street sign standards adopted by the City of Lathrop and match the overall color and design theme used in the Mossdale Landing project. Signage shall conform to the City’s requirements for traffic regulatory signs and posts. Street signs require 8’ vertical clearance. Consider shrub and tree location and spacing to avoid obstructing signs.

Residential Signage/Entry Monuments

Project gateway signage at River Islands Parkway and Street W shall receive the most significant and elaborative signage and documentation. Being an important entry into Mossdale Village, signage should establish a strong sense of entry. Materials and color for this location will set a precedent for the entire project. Signage features should welcome visitors and residents to Mossdale Landing West and Mossdale Village.

Community signage for Mossdale Landing West should be incorporated into entry columns or walls. If plaques are used, they shall be either precast concrete or metal. Contrasting color and materials of plaques will add visual interest and emphasis on the plaque. Signage shall be placed at all four corners and may be illuminated by concealed ground mounted lights or surface mounted back lighting. The maximum sign panel is 8 square feet unless an appropriate alternative is otherwise approved by the City.

Neighborhood Entry Signage

Neighborhood entry signage indicates entry into a specific neighborhood. These signs and monuments shall articulate and establish neighborhood identity and be integrated into entry columns or walls. Signage may incorporate project name, logo or emblem.

Park Signage

Neighborhood parks shall be identified by signage at major street intersections. Signs should clearly identify park name and design should reflect the theme of the park.

Model Homes Signage

Model homes signage includes signage that provide sales information to potential homeowners. They shall comply with the following:

- No more than two on-site subdivisions signs are permitted at subdivision entrance.
- Sign may be freestanding or wall mounted.
- Maximum area of sign shall be 32 square feet.
- Maximum height of a freestanding sign is 8' and shall be setback at minimum 5' from the right of way. Sign should not impair the vision of vehicles driving into or out of the site. Signs mounted on an entry or community wall may not extend above the top of the wall.

Community Off-Site Subdivision Signage

Community off-site subdivision signage provide sales and directional information to potential buyers. All off-site subdivision signage shall be approved by the City and comply with the following:

- Be freestanding and contain no more than 2 sides
- Maximum height 12', maximum width 8'
- Shall not be located within 1/8 mile (660 feet) from any other community off-site subdivision signage
- Any community off-site subdivision signage shall only advertise for Mossdale Landing West
- Information on signage shall be limited to subdivision name, logo, and directional arrow or directions to Mossdale Landing West.
- Signage may require permits. Home builder shall check with Building Department before erecting signage. Signage shall not be erected until information is advertised on the sign.
- Sign shall be removed within 30 days if no model homes or lots are available for viewing or sale.

Other Real Estate Sales/Leasing Signage

Display of signs for advertising real estate sales, leasing or rental opportunities shall be limited to one per property with a maximum of 8 square feet in area. Real estate sale/listing signs shall be removed within thirty(30) days of settlement of property.

Garage/Yard Sales Signage

Garage or yard sale signs shall comply with the following:

- Sign shall not exceed 4 square feet in area.
- May not be erected more than 2 days prior to event .
- One(1) sign shall be allowed on-site.
- All signs must be removed within one(1) day after the close of garage or yard sale.

Other signage not mentioned above shall require approval of the City.



EXAMPLE MODEL HOMES SIGNAGE



EXAMPLE OFF-SITE SUBDIVISION SIGNAGE

PAGE INTENTIONALLY BLANK



4

PUBLIC SERVICES



PUBLIC SERVICES

Communities require an array of public services and facilities that address the recreational, educational, governmental and emergency response needs of Mossdale Landing West. These facilities may include but are not limited to parks, fire, law enforcement, solid waste services and more. The City and other responsible agencies should review plans to ensure adequate public facilities and improvements are provided.

4.0 Fire Protection

The Lathrop-Manteca Fire District(LMFD) provides fire protection and emergency services for the City of Lathrop and consists of five fire stations. The closest station to Mossdale Landing West is Fire Station 34 located within a half mile of the community.

The LMFD is an independent special district governed by a five member Board of Directors who are elected to server alternating four-year terms. Each station is staffed with an Engine and Rescue company with a shift Battalion Chief Officer for oversight of on duty companies. Staff is available 24 hours a day at each station. The District's Headquarters is located at River Islands Fire Station 35. LMFD includes forty-two uniformed employees and 4 office staff employees. In addition to career personnel, LMFD has a roster of twenty-five reserve firefighters.

In addition to emergency response, the LMFD review proposed development projects and street networks to ensure accessibility for fire engines and other emergency response functions, review and enforce Fire and Life Safety Laws and promote community awareness of crime and safety among local neighborhoods. Community awareness and education includes CPR training, youth programs, working with schools, smoke detector installation for elderly and more.

4.1 Law Enforcement

As part of the City’s effort to provide the community with high-quality public safety services, facilities, and technology that protects against illicit activities and crime the City established its own Lathrop Police Department. In 2022, the City of Lathrop officially opened the Police Department to the public. Per the General Plan, Lathrop Police Department is tasked with several areas of responsibility including crime prevention, review of new development applications, community awareness and more. Refer to the Public Facilities and Services section of the General Plan for more details.

A new station located at River Islands is roughly half a mile away from the Mossdale Landing West community. The facility serves as the base of operations for Lathrop’s Police Department and as of June 2022, has forty-three sworn officers and seventeen non-sworn professional personnel. Staff positions include but not limited to police officer, HR analyst, police records assistance, property and evidence manager and technician, community services supervisor and officer, animal center supervisor and assistants, crime and intel analyst, etc. Based on population estimates from the United States Census Bureau, the City currently has roughly one sworn officer per 962 residents. The City of Lathrop aims to continually improve community safety, shape policing philosophy, and service priorities based on community needs.



4.2 Solid Waste & Recycling

The City of Lathrop is dedicated to maintaining safe and clean space for residents and visitors. As part of this effort, the Lathrop General Plan, includes policies and goals for solid waste and recycling services. Mossdale Landing West will be serviced by the City’s solid waste provider for any solid waste, recycling and green waste disposal needs. At the time of writing, the City holds an exclusive contract with Republic Services for any solid or green waste and recycling for residential and commercial areas.

The City utilizes a three cart system for the collection of solid waste and recycling to encourage the reduction of waste. Collection services are provided on a consistent basis and can be checked by visiting the City’s website. Residents are required to sign up and pay fees for the service.

Refer to Section 8.16 of the Lathrop Municipal Code for rules and regulations that pertain to garbage collection and disposal. The section includes a list of hazardous materials, rules against burning and burial of solid waste, waste collection and transportation and any restrictions related to it, collection schedule, permit requirements, related rates, and other related fees or requirements pertaining to solid waste and recycling.





4.3 Parks

As discussed, Mossdale Landing West will be a mixed-use community that incorporates amenities such as parks and open space to enrich both the project and the quality of life for its residents and visitors.

Low Density Residential uses are consistent with the General and Specific Plans, with the slight acreage difference due to the realignment of major streets and intersections. Proposed Single Family units are within their appropriate density ranges as required by the General Plan. Roadway realignments are all in conformance with the Specific Plan. The adjustments occurred due to engineering design criteria for the roadways. The Specific Plan alignments are generalized in nature for planning purposes with the understanding that once true engineering studies are completed, some realignments would occur.

Mossdale Landing West will be a diverse and livable community. The project has been designed to reflect and build upon the heritage and visual character of the area. Neighborhoods have been created to provide a wide mix of housing opportunities in various lot sizes and architectural styles. Park space have been sited to be centrally located in the community so as to provide recreational opportunities.

Park and open space acreage has been increased both in size and quantity to afford greater recreational amenities and open areas to community residents and guests. These areas of greenery and trees will reinforce the community character and identity. The levee area has been maintained as a continuous open space area to link with future segments of a regional open space corridor to be designed later by the City of Lathrop.

The close proximity of these uses will reduce vehicular traffic by encouraging walking and bicycling. The community will be pedestrian oriented, with a connectivity of sidewalks and trails designed throughout. Parkways may be provided to separate the pedestrian from vehicular traffic. Streetscape elements such as lighting standards and street trees have been selected to provide human scale and enhance the community theme.

4.4 Maintenance CFD

Infrastructure Financing Districts

- 1. Special Taxes such as Mello Roos Community Facilities Districts (CFD)
- 2. General Obligation Bonds
- 3. Revenue Bonds
- 4. Impact Fees
- 5. Private Developer Financing
- 6. Financing of Ongoing Operation and Maintenance (this could include Property Taxes, Transient Occupancy Taxes and Sales Taxes, User Fees, and Special Assessment resulting from the formation of a Landscaping and Lighting District)

Numerous financing mechanisms may be required to facilitate and implement the development and operation of major infrastructure items and essential community facilities. The project shall be responsible for financing all capital improvements and providing a mechanism for the funding of their future municipal operations and maintenance.

4.5 Schools

The Mossdale West project does not include establishing a school within the community. Mossdale Elementary School which was built during the development of Mossdale Village is the closest elementary school to Mossdale Landing West. The Mossdale Landing West community falls under the Manteca Unified School District (MUSD) boundary. MUSD will determine existing school capacity and assign where children will attend. Lathrop High School is the closest high school for children attending school between grades nine through twelve.

PAGE INTENTIONALLY BLANK



5

INFRASTRUCTURE



INFRASTRUCTURE

5.0 Introduction

The City of Lathrop approved and adopted three separate master plans written by EKI Environment and Water in December 2019. These master plans focused on the City’s recycled water, wastewater and water systems as a part of the Integrated Water Resources Master Plan (IWRMP) Update, which was an all-inclusive update to the City’s Potable Water System, Wastewater System and Recycled Water System Master Plans. Multiple City departments collaborated on the IWRMP Update to provide a cohesive structure to support forthcoming utility operations and the development and implementation of the Capital Improvement Project (CIP).

On December 10, 2002, the City approved the “Project Area Drainage Plan for Mossdale Landing” (Drainage Plan), which was later revised in September 2003. Then, in June 2015, the City adopted the “Multi-Agency Post-Construction Stormwater Standards Manual”. The purpose of this document was to deliver uniform direction for municipal workers, developers and builders in executing the requirements stipulated in the Statewide Small MS4 National Pollutant Discharge Elimination System (NPDES) Phase II permit. The City has also implemented a Stormwater Quality Master Plan, which was adopted in 2004.

5.1 Storm Drainage and Water Quality

Mossdale Landing West’s storm drain system will be developed in alignment with the Mossdale Landing Master Drainage Plan, which establishes objectives, hydrology data, and standards for 100-year flood protection, along with guidelines for the design, construction, and permitting of the San Joaquin River outfall.

According to the Drainage Plan, the Mossdale Village drainage shed is divided into six sub-sheds with a combined area of 912 acres. The Drainage Plan initially assigns Mossdale Landing West, encompassing approximately 208 acres, to the M3 sub-shed. However, a recent analysis by PACE determined that the M3 sub-shed lacks adequate capacity, necessitating a redirection of drainage northward to the M1 sub-shed.

The existing M1 sub-shed operates independently with its own pump station, stormwater quality basin or vault, and flood control detention basin. For Mossdale Landing West, underground detention solutions are proposed to manage runoff, in accordance with the 2022 City of Lathrop Design and Construction Standards. These standards ensure all facilities meet the minimum criteria for city acceptance and maintenance requirements. Each sub-shed is required to treat the first flush storm event, which is the volume of water equal to the 85th percentile of a 24-hour storm event. The existing pumps will begin to discharge water to an existing single outfall at the San Joaquin River (up to 30% of the peak discharge rate per the MBK Technical Memorandum dated

June 2002) once the first flush event has been treated. After the rain event is over, the pumps will continue to direct water to the river; however, if the San Joaquin River rises to a base flood level of 21.0 feet, the pumps will shut off until the water level in the river subsides. For further technical specifications and compliance details, refer to the Drainage Plan.

The storm drain lines in each individual residential street in Mossdale Landing West will drain in gravity pipelines and connect to the existing pipe system at Barbara Terry Boulevard and Marsh Road intersection. Water will then travel via gravity to the existing pump station located in the southwest corner of the Park West which will eventually pump the water into the San Joaquin River. Upgrades to the existing pump and storm drain system will be determined.

If an interim storm drain solution is required, a temporary detention basin can be constructed near the northern border of the site to hold water until it can be slowly released to enter the existing storm drain system.

In order to meet the requirements of the NPDES General Permit for Stormwater Discharges from Small MS4s, the City has prepared a Stormwater Management Plan (SWMP) and adopted the 2015 "Multi-Agency Post-Construction Stormwater Standards Manual". Because it is likely to undergo elevated population growth, the City must also adhere to the supplemental provisions of Attachment 4 of the General Permit, which contains design standards and receiving water restrictions that must be incorporated into the design and installation of infrastructure associated with new development. According to the General Permit, both structural and non-structural Best Management Practices (BMPs) for post-construction must be installed for any new development. Structural BMPs capture and treat the first flush runoff. Examples include grassy swales, stormwater quality basins and underground vaults. To help guarantee the proper continuing operation and maintenance of these BMPs, operations and maintenance (O&M) manuals and recommended maintenance schedules are required. Examples of non-structural BMPs include good housekeeping and employee training.



5.2 Domestic Water

The water system for Mossdale Landing West will be designed and constructed according to the City’s 2019 “Water System Master Plan” (WSMP), as well as the City’s 2020 Urban Water Management Plan(UWMP). Table 5.1 shows the predicted water demand for Mossdale Landing West at full build out.

TABLE 5.1 ESTIMATED WATER DEMAND FOR MOSSDALE LANDING WEST

LAND USE	ACREAGE	UNITS	WATER DEMAND FACTOR ¹ (GPD/DU)	WATER DEMAND FACTOR ¹ (GPD/AC)	AVERAGE DAILY WATER DEMAND (GPD)	WATER DEMAND (AFY)
Low Density Residential	-	829	330	-	273,570	306.44
Total					273,570	306.44
¹ Per City of Lathrop 2020 Urban Water Management Plan ² AFY=acre-ft/year						

According to the WSMP, water that services Mossdale Landing West can originate from one of the following sources:

1. Imported surface water from the Stanislaus River through the South County Water Supply Program (SCWSP) that is operated by the South San Joaquin Irrigation District (SSJID), or
2. Groundwater production wells owned and operated by the City.

5.3 Wastewater Treatment Options

The wastewater system for Mossdale Landing West will be designed and constructed according to the City’s 2019 “Wastewater System Master Plan” (WWSMP). Wastewater from the Mossdale Landing West site will be directed via a gravity system to the existing Mossdale Pump Station, located near the northwest corner of the intersection of River Islands Parkway and McKee Boulevard. From there, it will travel via force main to be treated at the City-owned Lathrop Consolidated Treatment Facility (CTF), which is located on S Howland Road, northeast of the I-5/ 120 Interchange. Upgrades may be required to the pump station and the downstream system to accommodate wastewater from the Mossdale Landing West site.



Table 5.2 shows the predicted amount of wastewater that will be produced upon the full build-out of Mossdale Landing West.

TABLE 5.2 ESTIMATED WASTEWATER DEMAND FOR MOSSDALE LANDING WEST

LAND USE	ACREAGE	UNITS	WASTEWATER FLOW FACTOR 1 (GPD/DU)	WASTEWATER FLOW FACTOR 1 (GPD/AC)	AVERAGE DAILY WASTEWATER DRY WEATHER FLOW(GPD)
Low Density Residential	-	829	200	-	166,000
Parks	13.0	-	-	55	715.0
Total					166,715

¹Per City of Lathrop 2020 Wastewater System Master Plan

5.4 Recycled Water

The City’s 2019 Recycled Water System Master Plan (RWSMP) provides guidance for the design and implementation of the City’s recycled water system, which supports the removal of effluent created by the City-owned Lathrop CTF. The recycled water system is composed of seven agricultural land application areas (LAAs), nine storage ponds, five pump stations and over thirty miles of pipeline. The use of recycled water in the City is regulated by Waste Discharge Requirement (WDR) Order Number R5-2016-028-01, which allows for the following uses:

- Irrigation of LAAs,
- Irrigation of public landscape areas, including open spaces, pond berms, parks and roadway medians, and
- Percolation into the ground at Percolation Basin 1 (PB-1) on the east side of the City.

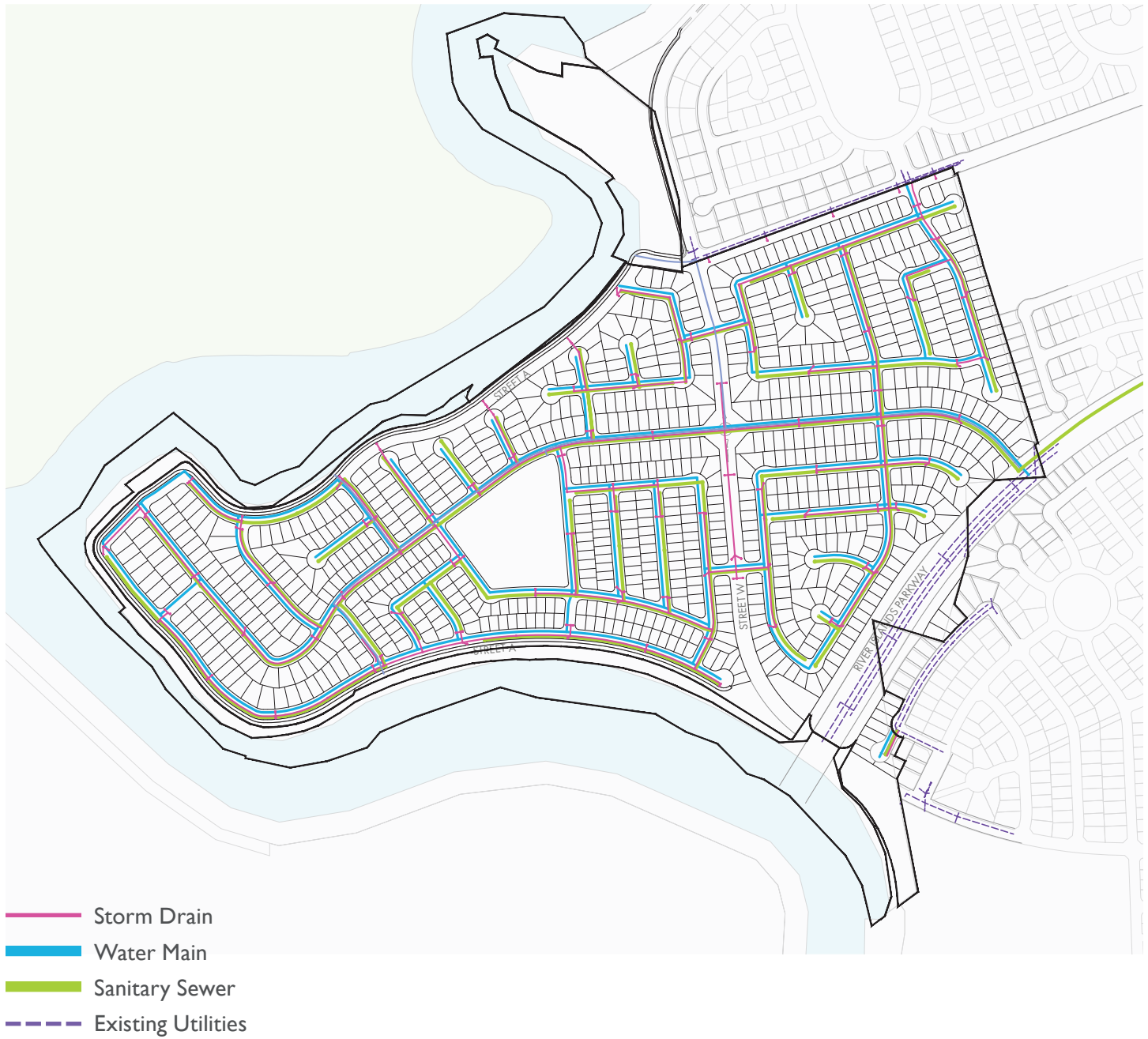
Infrastructure for the recycled water system has been installed in the Mossdale Landing West vicinity (including in River Islands Parkway and Barbara Terry Boulevard); however, the pipes are currently inactive.

Table 5.3 shows the annual consumption of recycled water of the Mossdale Landing West development upon buildout.

TABLE 5.3 ESTIMATED RECYCLED WATER DEMAND FOR MOSSDALE LANDING WEST

LAND USE	ACRES	APPLICATION RATE (AC-IN/AC/YR)	PERCENT IRRIGATED	ANNUAL DEMAND (AC-IN/YR)
Neighborhood Park	6.2	55	100%	341
Parkland Dedication	2.0	55	100%	110
Riverfront Park	4.8	55	100%	264
Public Landscaping	9.3	55	100%	511.5
Landscaped Lots	3.0	55	100%	165
Total	25.3	-	-	1,391.5

FIGURE 5.1 MOSSDALE LANDING WEST UTILITIES MAP



5.5 Dry Utilities

All mechanical equipment, including air conditioners, gas regulators, telephone/cable.TV pedestals, etc. shall be located in visually unobtrusive locations, screened from view from surrounding areas and baffled for noise attenuation where necessary. Roof top equipment must be hidden in mechanical wells or screened by mechanical enclosures. Satellite dishes and solar panels shall be integrated as best as possible, but should be located in visually unobtrusive locations and screened from views from residential areas.

5.6 Electricity and Natural Gas

Pacific Gas and Electric provide both electrical and natural gas utilities to Mossdale West residents.

5.8 Telecommunications

Mossdale West residents have several options for telecommunications providers. Cable and/or internet providers include AT&T or Comcast while Verizon and AT&T are the main providers for phone coverage.

PAGE INTENTIONALLY BLANK



6

IMPLEMENTATION

IMPLEMENTATION

6.0 Entitlements

The Mossdale Landing West project seeks the following entitlements: Specific Plan and Vesting Tentative Map, the cancellation of the Williamson Act, Development Agreement, and the project CEQA.

6.1 Financing Mechanisms

The City will require the Mossdale Landing West project to construct certain infrastructure to provide access and utilities to the site, the details of which are included in the Infrastructure section of this document. To guarantee that the project has sufficient funding to carry out these requirements, various financing methods may be employed, as described in Section VI-C of the West Lathrop Specific Plan (WLSP), including:

- Infrastructure Financing Districts,
- Special Taxes,
- Special Assessments,
- General Obligation Bonds,
- Revenue Bonds,
- Impact Fees,
- Third Party Assistance,
- Private Developer Financing, and
- Financing of Ongoing Operation and Maintenance.

Table VI-3 of the WLSP summarizes the up front and on-going funding sources for the Mossdale Landing West utilities as described in the Infrastructure section of this document. It also includes funding for roads and streets (both on-site and off-site), trenched utilities, parks, open space and in-tract construction.

Private developer financing will account for the majority of Mossdale Landing West's on-site improvements. Existing Barbara Terry Boulevard and River Islands Parkway will be built out to their full right-of-way width, and existing Street W will be extended under River Islands Parkway with full right-of-way improvements.

There approximately 16.6 acres of parks associated with the Mossdale Landing West project: one neighborhood park in the center of the development, a linear park and two acres of land that will be dedicated for park development south of River Islands Boulevard. Additionally, there is 0.6 acres of landscape lots surrounding the Street W roundabout and on either side of the intersection of Street C and River Islands Parkway that will be built and initially funded by the developer. However, since these areas will be public facilities, the construction costs may be reimbursed by the City. Ongoing operations and maintenance costs will be the financial responsibility of the City.

6.2 Financial Plan

As mentioned previously, the upfront costs for construction associated with the Mossdale Landing West project are generally the responsibility of the developer. The ongoing operation and maintenance of public facilities (roads, utilities, parks, etc.) will be funded by the City, as appropriate.

The Developer will prepare a Fiscal Impact Analysis report, Financial Plan, and Conditions of Approval for the Vesting Tentative Map prior to approving the Mossdale Landing West Final Map. This thorough plan will identify suitable funding sources for construction and maintenance of public infrastructure, community facilities, and other public services that are required to develop the area.

The Financing Plan will be executed according to the following principles as delineated in the WLSP unless other arrangements are made between the City and the developer:

1. Existing residents in the City shall not incur costs for infrastructure or services required to serve the proposed project unless a direct benefit can be demonstrated. All on-site and off-site costs associated with providing municipal services shall be paid for through the project budget.
2. The City will establish appropriate financing mechanisms to cover the cost of municipal services. In addition, the City may assist in securing the financing necessary for capital infrastructure construction and maintenance. These mechanisms include, but are not limited to the following:
 - a. The establishment of a Lighting and Landscape District, maintenance district, and user charges for operation and maintenance purposes.
 - b. The establishment of Assessment Districts, Benefit Districts, Community Facilities Districts, Infrastructure Financing Districts and Joint Power Arrangements for capital construction.
3. The City may enter into a Joint Power Agreement with the County, State or any other appropriate governmental agency that will facilitate the financing of necessary infrastructure improvements.
4. The City agrees to establish reimbursement mechanisms if a development pays for infrastructure that exceeds what is needed by that development.
5. The City shall consider implementing per-unit fees to accommodate financing infrastructure improvements or the reimbursement of costs fronted by the developer.
6. The City and the developer will actively pursue outside funding for regional infrastructure improvements that benefit the project.
7. The City will assist the developer of the plan area in obtaining private and public financing for both on- and off-site improvements.

6.3 City Approval Process

The review and approval of the Mossdale Landing West project will be conducted by the City of Lathrop Community Development Department, which will include a Specific Plan, Vesting Tentative Map, cancellation of the Williamson Act, Development Agreement, a project CEQA, Building Permit review, Improvement Plan review and a Final Map.

The City of Lathrop Zoning Map accompanies the Lathrop Municipal Code Title 17: Zoning. The current Zoning Map shows the Mossdale Landing West project area as being zoned RL-MV per the City's Zoning Consistency Project.

Specific Plan

The Specific Plan provides a framework of development and project implementation for use by the City, developers and builders, which includes street and design standards and guidelines, detailed land uses, infrastructure, site planning, architecture, landscape. The approval of this document satisfies the requirements of the City's Specific Plan process.

Architectural Design Review

Architectural Design Review(ADR) is a discretionary permit and will be required prior to building permit. The purpose of the ADR is to confirm that the proposed plans for this project are in keeping with the policies and guidelines set forth in the General Plan and the Mossdale Landing West Specific Plan. The ADR will include an administrative approval review of Model Home Complexes. The City requires projects to meet specific standards with respect to architectural styles and signage, landscape and design themes. The Architecture Design Review discretionary permit is reviewed and approved by the Community Development Director.

Vesting Tentative Map

Also referred to as a Tentative Subdivision Map, a Vesting Tentative Map (VTM) will be submitted to initiate the process of subdividing the Mossdale Landing West project. The VTM design will be governed by the Subdivision Map Act, the City of Lathrop Subdivision Ordinance, the WLSP, the Specific Plan and the infrastructure master plans (see the Infrastructure section of this document). The VTM will be reviewed by the Planning Commission and approved by the City Council.

Williamson Act Cancellation and Development Agreement

Mossdale Landing West falls under the City's Sub-Plan Area #2 as indicated in the City of Lathrop General Plan and is a part of the West Lathrop Specific Plan.

All of Mossdale Landing West falls under the Williamson Act and will require existing contracts to go through the process of cancellation and non-renewal.

Cancellation of the Williamson Act is provided in Sections 51240-51287 of the Government Code. The state law requires those who wish for non-renewal, to file a Notice of Non-Renewal signifying intent to not renew the contract and file a petition for cancellation with the Lathrop City Council. The Lathrop City Council must find that the cancellation is consistent with the purposes of the Williamson Act and furthers public interest to approve the cancellation. Once approved, the land may continue to be used for agricultural purposes up until the development of land requires discontinuation.

Project CEQA

An environmental review in compliance with the California Environmental Quality Act (CEQA) will be required for the Mossdale Landing West project.

Building Permit Review

City staff will review construction documents for compliance as part of the building permit process during the Final Map stage.

Improvement Plan Review

Improvement plans will be drafted to demonstrate the grading, design and installation of all public and private, on- and off-site infrastructure (utilities, streets, parks, residential lots, etc.) associated with the Mossdale Landing West project. These plans must be approved by City staff prior to the granting of construction permits and the approval of the Final Map.

Final Map

Upon meeting the conditions of the VTM, the approval of improvement plans and the finalization of all agreements between the City and the developer, the City can approve the Final Map for Mossdale Landing West in accordance with the Subdivision Map Act as implemented by the City Subdivision Ordinance. If the project's Final Map closely reflects the lotting pattern shown in the VTM and provides the appropriate residential density for a low density residential zone, the Map will be deemed consistent with the Mossdale Landing West Specific Plan.

Subsequent Environmental Review

Changes to Mossdale Landing West community after review and approval will require a subsequent environmental review. A project-level EIR is currently being prepared by De Novo Planning Group

6.4 Amendment Process

It is expected that updates will need to be made to the Specific Plan through the development cycle of a community. The City recognizes two categories of amendments: Minor Amendments and Major Amendments. A Minor Amendment is accomplished through an administrative change to the Specific Plan with the approval of the Community Development Director without notice or a public hearing. Any other proposed modifications are classified as a Major Amendment and must receive City Council approval. Both Minor and Major Amendments are to be consistent with the General Plan, and the Mossdale Landing West Specific Plan.

Minor Specific Plan Amendment

Changes to text or graphics that are in keeping with the original Specific Plan and with the flexibility mechanisms of the Specific Plan are considered Minor Amendments and may receive administrative approval from the Community Development Director. No public hearing is required; however, Minor Amendments are subject to an appeal to the Planning Commission. Examples of a Minor Amendment request can include:

- Modifications to plant palettes,
- Changes to permitted building materials,
- Editorial corrections to text or exhibits,
- Amendments to text or graphics to align with other laws,
- A retroactive change to text or graphics to align with existing conditions and/or previous City development project approvals,
- Realignment to walking trails,
- Alterations to fence types, materials, or locations,
- Any other comparable revision that is in harmony with the purpose and intent of a Minor Amendment as determined by the Community Development Director, or
- The inclusion of new architectural styles (which shall be submitted with a narrative describing the style, a schematic drawing, and a graphic representation of the elements which characterize the proposed style).
- Slight increases and decreases in overall unit count

Major Specific Plan Amendment

Alternatively, if the Community Development Director considers a request for a change to be more than a minor, administrative change, the request would be deemed a Major Amendment and must be approved by the City Council. If the Major Amendment is substantial enough, it could trigger a simultaneous change to the General Plan. Any request for changes to text and graphics that does not fall into one of the categories listed in the previous section would represent a Major Amendment, such as the relocation of a neighborhood park. Major Amendments must be adopted using the same procedure as was used when the Specific Plan was originally adapted. An amendment or amendments to this Specific Plan shall not require a concurrent General Plan Amendment unless it is determined by City Staff that the proposed amendment would substantively affect the General Plan goals, objectives, policies or land use program.

6.5 Differences Between the West Lathrop Specific Plan and the Mosssdale Landing West Specific Plan

Land Uses:

The 2002 West Lathrop Specific Plan exhibits and text describe the Mosssdale Landing West area as having a mix of waterfront commercial, single family residential, and open space; however, the proposed project alters the land use to just Public (P) and LD. These uses have been evaluated and revised to better address the needs within the overall West Lathrop Specific Plan. An amendment to the WLSP has been initiated to accurately portray the Mosssdale Landing West project.

Street Sections:

The roads associated with Mosssdale Landing West match the street sections shown in the WLSP except for the local street section shown in Proposed Road Section E in Figure IV-7 in the WLSP. The revised local street section has a 46' right-of-way width with a 5' wide sidewalk between the right-of-way line on both sides. The original local street section is a 56' wide right-of-way with a 5' wide landscape strip adjacent to the curb and a 5' wide sidewalk between the landscape strip and the right-of-way on both sides.

For fully dimensioned street sections, see Figure 2.6 in the Landscape Architecture Standards section of this document or the Mosssdale Landing West VTM.

In addition, the development scope of Mosssdale Landing West includes the full build out of Barbara Terry Boulevard and River Islands Parkway adjacent to the site and the extension of Towne Centre Drive into Street W under River Islands Parkway and through the site. All three streets will have full frontage improvements. The fully dimensioned street sections for these three roads are shown in Figure 2.6 in the Landscape Architecture Standards section of this document and on the Mosssdale Landing West VTM. The current version of the WLSP does not show either of these; however, the sections for Barbara Terry Boulevard and the Towne Centre Drive extension will be included in the amended WLSP.

TABLE 6.0 MOSSDALE LANDING WEST - STREET SECTIONS

CLASSIFICATION	RIGHT-OF-WAY-WIDTH	FACE OF CURB TO FACE OF CURB	LANDSCAPE STRIP WIDTH	SIDEWALK WIDTH
Specific Plan - Residential Streets	62'	36'	8'	5'
VTM - Interior Streets	46'	36'	-	5'
VTM- Street A (54' ROW)	54'	36'	Varies	5'
VTM- Street A (49' ROW)	49'	36'	Varies	5'
WLSP - Local Street	56'	36'	5'	5'

TABLE 6.1 RIVER ISLANDS PARKWAY - STREET SECTION

CLASSIFICATION	RIGHT-OF-WAY-WIDTH	FACE OF CURB TO FACE OF CURB ¹	EX. MEDIAN WIDTH ²	LANDSCAPE STRIP WIDTH ^{1,3}	SIDEWALK WIDTH
Specific Plan - River Islands Parkway	156'	31'	16'	8' + 23'	8'
VTM - River Islands Parkway	156'	31'	16'	8' + 23'	8'
WLSP	Not Shown	Not Shown	Not Shown	Not Shown	Not Shown

¹The west side of River Islands Parkway will be constructed to match the existing section on the east side. The existing east side improvements will remain in place.
²Existing median to remain in place.
³The two landscape strips are separated by the sidewalk.

TABLE 6.2 STREET W - STREET SECTION

CLASSIFICATION	RIGHT-OF-WAY-WIDTH	FACE OF CURB TO FACE OF CURB	LANDSCAPE STRIP WIDTH ¹	SIDEWALK WIDTH
Specific Plan - Towne Centre Drive	80'	50'	Varies	5'
VTM - Street W	80'	50'	Varies	5'
WLSP ²	Not Shown	Not Shown	Not Shown	Not Shown

¹There are two landscape strips of varying width that are separated by the sidewalk. The total width will be 10'.
²The amended version of the WLSP will include a section for Towne Centre Drive that will match those shown in the VTM and Specific Plan.

TABLE 6.3 BARBARA TERRY BOULEVARD - STREET SECTION

CLASSIFICATION	RIGHT-OF-WAY-WIDTH	FACE OF CURB TO FACE OF CURB	LANDSCAPE STRIP WIDTH ¹	SIDEWALK WIDTH
Specific Plan - Barbara Terry Boulevard	90'	50'	5' + 10'	5'
VTM - Barbara Terry Boulevard	90'	50'	5' + 7'	5'
WLSP ²	Not Shown	Not Shown	Not Shown	Not Shown

¹The two landscape strips are separated by the sidewalk.
²The amended version of the WLSP will include a section for Barbara Terry Boulevard that will match those shown in the VTM and Specific Plan.

PAGE INTENTIONALLY BLANK



APPENDIX

PAGE INTENTIONALLY BLANK



APPENDIX “A” ARCHITECTURAL STYLES

The following pages describe the architectural styles for low density that will be used throughout Mossdale Landing West in accordance with the Mossdale Landing South Urban Design Concept . These pages are directly copied from the Mossdale Landing South UDC with the intention of matching architectural styles to create unity between the existing Mossdale Village community and the planned development of Mossdale Landing West. These descriptions are open to interpretation but serve as the basis of design for the community.





SPANISH

The style combines the entire history of Spanish architecture, which may be of Moorish, Byzantine, Gothic, or Renaissance inspiration lending an unusually rich and varied series of decorative precedents. The 1915 San Diego Exposition increased the popularity of the style through designs by Betram G. Goodhue and Carleton M. Winslow and it was subsequently refined by Montecito architect, George Washington Smith.

Form and Mass

Usually simple one or two-story volumes with a low-pitched roof of little or no eave overhang. Rear colonnades and porches are covered by this principal roof. The main roof is typically gabled and is often combined with wings of either gabled or hip roof forms. Shed extensions of the roof are common at entryways or projecting windows.

The façades walls are massive and dominate the typically asymmetrical, deep-set punched openings. Only the doors and the principal windows are arched. Oftentimes, the principal windows are triple arched with the center window large in scale.

Materials and Details

Roofs are red in either Mission tile or Spanish Tile. Walls are off-white or creamy beige stucco with little or no texture. A variety of ornate, colorful detailing accompanies the main doors and focal windows (such as spiral columns, carved stone work, etc.). Heavy wood entry doors can include tiny multiple paned openings. Similarly, focal windows contain the same multiple paned ornate glazing. French doors often open to the rear covered porches and colonnades.



ITALIAN

The Italian revival of the late 1800's is credited to the New York Villard Houses of McKim, Mead & White. This style accurately mimics the Italian Renaissance. Post World War I improvements in masonry veneering made authenticity more possible.

Form and Mass

Traditionally, simple boxlike forms can brace either a subordinate projecting central wing or two side wings. These buildings are almost typically symmetrical in both their form and openings. The roof, including recessed entry porches, is typically hipped with a substantial eave. Modern interpretations break down the traditional box, but maintain the changeable detailing and dominant two-story wall massing.

Materials and Details

The roof is s-tiled. The eave is typically boxed and supported by frequent decorative brackets integrated with a strong cornice.

The walls are masonry veneer or either rough or smooth stucco. Colors are most often off-whites, creams, or beiges. Openings are deep-set. First floor openings are arched and the second floor openings are squared up against the cornice. Shutters and cast-iron railings and balconies are common.



CRAFTSMAN/BUNGALOW

The rejection of contemporary Victorian detailing and a humanizing of the new machine aesthetic generated the English Arts and Crafts movement of the late 19th century and the craftsman house. The architects, Greene and Greene, championed the style in the United States and furthered the intricate wooden detailing with traditional Asian woodworking.

Form and Mass

Low-pitched, gable roofs (seldom hipped) with wide exposed rafter tail eaves and rakes cover simply raised boxy forms. The gable ends can be front facing or side facing, and sometimes may be combined in a crossed-gabled form. Porches are typically integrated into the roof form.

Materials and Details

In response to the ornate Victorian detailing, these buildings strove to express the building elements in a tasteful handmade way. This expression occurred throughout all the elements. Strong and crafted barge rafters are supported by projecting roof beams or knee braces. Porches are varied in detailing, but all contain simple forms of columns and beams supported by more massive piers continuing from footing to above-rail height. Windows and doors are wide, wood-cased elements often with asymmetrical panes. The roof material is typically wood shingle or asphalt composition. Siding is most often a variety of wood types with accent of stone. Stucco is seldom used. Colors are often earth tone with some pastels and low in contrast.



ENGLISH COUNTRY

The rich history of English vernacular architecture began with the Norman conquest of 1066. Political and economic stability brought the first permanent housing to England. Centuries of vast folk influences have created a deep and rich character in the English cottage, one that holds great popularity in America. The ability to recreate this style was greatly enhanced when veneer techniques were improved in the 1920's.

Form and Mass

Simple elongated one or two-storied boxes are often articulated by asymmetrical front and rear projecting wings. The length of the plan typically fronts the street and is side-gabled. The projecting wings are front-gabled and form an asymmetrical cross-gabled roof.

Materials and Details

The roof is typically a flat tile. Eaves are small and often boxed. Walls are typically stucco with wood and brick. Brick detailing can be included. The stucco walls are detailed similar to Tudor detailing. Timber lintels at openings and stucco infill of timber framing elements are typical. The dark-colored timbers are contrasted with creamy or off-white stucco to accentuate this detailing. Gable ends are often projected at the support line and corbel supports expressed below. Entry elements provide a change in detail, but still remain simple, not ornate.

APPENDIX “B” PLANT LIST

MOSSDALE LANDING WEST PLANT LIST

TREES

Botanical Name	Common Name	Height	Width	Water Use
<i>Acer rubrum</i> 'Redpointe'	Redpointe Maple	40-50'	20-30'	M
<i>Carpinus betulus</i> 'Fastigata'	European Hornbeam	35-45'	25-35'	M
<i>Cercis occidentalis</i>	Western Redbud	10-20'	10-20'	VL
<i>Ginkgo biloba</i> 'Autumn Gold' TM	Maidenhair Tree	35-40'	25-35'	M
<i>Koelreuteria paniculata</i>	Goldenrain Tree	20-35'	25-40'	M
<i>Lagerstroemia</i> X 'Natchez'	Natchez Crape Myrtle	20-30'	15-20'	L
<i>Malus</i> x 'Prairifire'	Prairifire Crabapple	15-20'	15-20'	M
<i>Pistacia chinensis</i> 'Keith Davey'	Keith Davey Chinese Pistache	25-40'	25-35'	L
<i>Planatus acerifolia</i> 'Bloodgood'	London Plane Tree	40-50'	25-30'	M
<i>Quercus coccinea</i>	Scarlet Oak	40'-50'	50'-70'	M
<i>Quercus lobata</i>	Valley Oak	50-75'	50-80'	L
<i>Quercus suber</i>	Cork Oak	30-60'	30-60'	L
<i>Ulmus parvifolia</i> 'Drake'	Drake Elm	50-70'	30-50'	M
<i>Zelkova serrata</i> 'Green Vase'	Sawleaf Zelkova	45-50'	30-40'	M

SHRUBS

Botanical Name	Common Name	Height	Width	Water Use
<i>Abelia</i> x <i>grandiflora</i>	Glossy Abelia	4-6'	4-5'	M
<i>Abelia</i> x <i>grandiflora</i> 'Kaleidoscope'	Kaleidoscope Abelia	2-3'	3-4'	M
<i>Anisodonteia</i> 'Tara's Pink'	Cape Mallow	6-8'	4-6'	L
<i>Cistus</i> x <i>pulverulentus</i> 'Sunset'	Magenta Rockrose	2-3'	6-8'	L
<i>Correa</i> 'Carmine Bells'	Red Australian Fuchsia	2-3'	4-6"	L
<i>Grevillea</i> x 'Noellii'	Grevillea	4-5'	4-5'	L
<i>Leucophyllum frutescens</i> 'Compacta'	Compact Texas Ranger	4-5'	4-5'	L
<i>Ligustrum japonicum</i> 'Texanum'	Wax Leaf Privet	6-8'	4-6'	L
<i>Myrtus communis</i> 'Compacta'	Dwarf Myrtle	2-3'	2-3'	L
<i>Nerium oleander</i> 'Little Red' TM	Little Red Oleander	3-4'	3-4'	L
<i>Nerium oleander</i> 'Petite Pink'	Petite Pink Oleander	3-5'	3-5'	L
<i>Olea europaea</i> 'Little Ollie' TM	Little Ollie Olive	6-8'	6-8'	VL
<i>Raphiolepis indica</i> 'Ballerina'	Ballerina Indian Hawthorn	2-3'	3-4'	M
<i>Raphiolepis umbellata</i> 'Minor'	Yedda Hawthorn	4-6'	3-4'	L
<i>Salvia microphylla</i> 'Hot Lips'	Hot Lips Sage	3-4'	3-4'	L
<i>Teucrium fruticans</i> 'Azureum'	Bush Germander	3-4'	4-5'	L
<i>Westringia fruticosa</i> 'Wynabbie Gem'	Wynabbie Gem Coast Rosemary	6-8'	4-6'	L
<i>Xylosma congestum</i>	Shiny Xylosma	8'-12'	10'-12'	L

ACCENT PLANTS

Botanical Name	Common Name	Height	Width	Water Use
<i>Agapanthus africanus</i>	Lily of the Nile	2-3'	3-4'	L
<i>Dietes</i> sp.	Fortnight Lily	2-3'	3-4'	L
<i>Hemerocallis</i> sp.	Daylily	1-3'	1-3'	M
<i>Hesperaloe parviflora</i> 'Brakelights' TM	Brakelights Red Yucca	2-3'	2-3'	L
<i>Limonium californicum</i>	Western Marsh Rosemary	1-2'	1-2'	L
<i>Salvia</i> sp.	Sage	varies	varies	L
<i>Tulbaghia violacea</i> 'Silver Lace'	Variegated Society Garlic	1-3'	1-2'	L

GRASSES

Botanical Name	Common Name	Height	Width	Water Use
<i>Lomandra longifolia</i> 'Breeze'	Dwarf Mat Rush	2-3'	2-4'	M

Muhlenbergia capillaris	Hairy Awn Muhly	3-4'	3-4'	L
Pennisetum apolecuridides 'Little Bunny'	Little Bunny Fountain Grass	1'	1'	L

GROUNDCOVER

Botanical Name	Common Name	Height	Width	Water Use
Coprosma kirkii	Kirk's Coprosma	1-3'	4-6'	L
Cotoneaster dammeri 'Coral Beauty'	Bearberry Cotoneaster	1-2'	4-6'	L
Juniperus horizontalis 'Prince of Wales'	Prince of Wales Juniper	1'	8-10'	L
Myoporum parvifolium 'Putah Creek'	Putah Creek Myoporum	1-2'	10-15'	L
Rosa x 'Noare'	Flower Carpet Red Groundcover Rose	1-2'	2-3'	M
Rosmarinus officinalis 'Prostratus'	Dwarf Rosemary	1-2'	4-8'	L

VINES

Botanical Name	Common Name	Height	Width	Water Use
Clytostoma callistegioides	Violet Trumpet Vine	15-25'		M
Ficus pumila	Creeping Fig	20-30'		M
Hardenbergia violacea 'Happy Wanderer'	Hardenbergia Vine	10-15'		M
Parthenocissus quinquefolia	Virginia Creeper	30-50'		M

RIVER ISLANDS PARKWAY

TREES

Botanical Name	Common Name	Height	Width	Water Use
Arbutus x 'Marina'	Arbutus Multi-trunk	20-40'	20-30'	L
Chilopsis linearis 'Timeless Beauty'	Desert Willow	15-20'	15-20'	VL
Koelreuteria paniculata	Goldenrain Tree	20-35'	25-40'	M
Quercus coccinea	Scarlet Oak	50-70'	40-50'	M
Quercus lobata	Valley Oak	50-75'	50-80'	L
Zelkova serrata 'Green Vase'	Sawleaf Zelkova	45-50'	30-40'	M

SHRUBS

Botanical Name	Common Name	Height	Width	Water Use
Abelia x grandiflora 'Kaleidoscope'	Kaleidoscope Abelia	2-3'	3-4'	M
Anisodonteia 'Tara's Pink'	Cape Mallow	6-8'	4-6'	L
Berberis thunbergii 'Atropurpurea'	Red Leaf Japanese Barberry	3-4'	3-4'	M
Ceanothus horizontalis 'Yankee Point'	Yankee Point Ceanothus	2-3'	8-10'	L
Ceanothus x 'Joyce Coulter'	Ceanothus Joyce Coulter	2-3'	10-15'	L
Cistus x purpureus	Orchid Rockrose	3-4'	4-6'	L
Coleonema pulchrum 'Sunset Gold'	Golden Breath of Haven	2-4'	4-6'	M
Nerium Oleander 'Petite Pink'	Petite Pink Oleander	3-5'	3-5'	L
Prunus caroliniana 'Bright 'N Tight'	Carolina Laurel	8-10'	6-8'	L
Rhaphiolepis umbellata 'Minor'	Yedda Hawthorn	4-6'	3-4'	L
Rosmarinus officinalis 'Blue Spires'	Rosemary	4-5'	2-3'	L
Salvia Microphylla 'Hot Lips'	Hot Lips Sage	3-4'	3-4'	L
Teucrium fruticans 'Azureum'	Bush Germander	3-4'	4-5'	L

ACCENT PLANTS

Botanical Name	Common Name	Height	Width	Water Use
Dietes bicolor	Fortnight Lily	2-3'	3-4'	L
Erigeron karvinskianus 'Profusion'	Santa Barbara Daisy	1'	3-4'	L
Hesperaloe Parviflora 'Brakelights' TM	Brakelights Red Yucca	2-3'	2-3'	L

Lavandula stoechas 'Otto Quast'	Spanish Lavender	2'	2-3'	L
Nandina domestica 'Gulf Stream'	Heavenly Bamboo	2-3'	2-3'	L
Penstemon heterophyllus 'Margarita BOP'	Margarita Bop Beardtongue	1-2'	1-2'	L
Tulbaghia violacea 'Variegata'	Variegated Society Garlic	1-3'	1-2'	L
Zauschneria californica	California Fuchsia	3-4'	3-4'	L

GRASSES

Botanical Name	Common Name	Height	Width	Water Use
Calamagrostis x acutiflora 'Karl Foerster'	Feather Reed Grass	4-6'	2-3'	L
Juncus patens	California Grey Rush	1-2'	1-2'	M
Muhlenbergia capillaris 'Pink Cloud'	Hairy Awn Muhly	3-4'	3-4'	L
Pennisetum apolecuridides 'Little Bunny'	Little Bunny Fountain Grass	1'	1'	L

GROUNDCOVER

Botanical Name	Common Name	Height	Width	Water Use
Cotoneaster dammeri 'Lowfast'	Bearberry Cotoneaster	1-2'	4-6'	L
Myoporum parvifolium 'Putah Creek'	Putah Creek Myoporum	1-2'	10-15'	L
Rosa x 'Noare'	Flower Carpet Red Groundcover Rose	1-2'	2-3'	M
Rosa x 'Noaschnee'	Flower Carpet White Groundcover Rose	1'-2'	3'-4'	M

APPENDIX “C” LATHROP MUNICIPAL CODES (LMC)

Applicable LMC for Fencing

Section 17.04.080

Fence, Open. “Open fence” means a fence, fifty percent (50%) or more of the vertical surface of which is open to the transmission of light, air, and vision.

Fence, Screened. “Screened fence” means a fence, ninety percent (90%) or more of the vertical surface of which is closed to the transmission of light, air, and vision.

Section 17.92.070

N. Fence and Wall Maximum Allowed Heights and Locations. Each fence and wall on a residential property shall comply with height limits and locations shown in the table below.

Location of fence or wall	Maximum Height
Within required front setback	3 feet
Within required street side setback < 5 feet from back of sidewalk	3 feet
> 5 feet from back of sidewalk	7 feet
Within required interior side and rear setback	7 feet
At the intersections of streets, alleys and driveways	3 feet

O. Fence and Wall Design and Maintenance Standards

1. Fencing Materials. Fences and walls shall be constructed of attractive, long-lasting materials (e.g., masonry, wood, tubular steel, or stone).

2. Prohibited Fencing Materials. Unless approved as a condition of approval or in conjunction with another entitlement, walls or fences of sheet or corrugated iron, sheet steel, concertina wire, or sheer aluminum are prohibited. Barbed wire fencing, concertina wire, serpentine wire, razor wire, and other similar fencing materials shall not be constructed or placed on top of a fence except where property is used for agricultural, open space, or industrial uses.

3. Maintenance. Fences and walls shall be continuously maintained in an orderly and good condition.

4. Temporary Fences. Nothing in this chapter shall be deemed to prohibit the erection of a temporary fence around construction project in compliance with the city adopted building code and other applicable provision of the municipal code.

APPENDIX “D” ADOPTED GENERAL PLAN CONSISTENCY TABLE

The Mossdale Landing West Specific Plan is intended to provide systematic development of the area consistent with the goals and policies of the City of Lathrop’s General Plan and those in the West Lathrop Specific Plan. Below is a list of goals and objectives that are applicable to the development and have been met in the Specific Plan.

General Plan Goal/Objective
Goal #1 Balancing the social and economic costs and benefits of urbanization
<ul style="list-style-type: none"> • Objective 1A • Objective 1B • Objective 1C • Objective 1D
Goal #2 Equal Opportunity
<ul style="list-style-type: none"> • Objective 2A • Objective 2C
Goal #3 Community Identity
<ul style="list-style-type: none"> • Objective 3A • Objective 3B • Objective 3C • Objective 3D • Objective 3E • Objective 3F • Objective 3G • Objective 3H • Objective 3I • Objective 3J • Objective 3K • Objective 3L • Objective 3P • Objective 3Q
Goal #4 Quality in the form, Design an Function of the Urban Area
<ul style="list-style-type: none"> • Objective 4D • Objective 4E • Objective 4G
Objective #5 Enhancing the Quality of Life
<ul style="list-style-type: none"> • Objective 5A • Objective 5C
Goal #6 Transportation Circulation/Traffic
<ul style="list-style-type: none"> • Objective 6A • Objective 6B • Objective 6C • Objective 6D

General Plan Goal/Objective
Goal #7 Seismic Hazards
<ul style="list-style-type: none"> • Objective 7A
Goal #8 Pubic Safety Hazards
<ul style="list-style-type: none"> • Objective 8B
Goal #9 Noise Hazards
<ul style="list-style-type: none"> • Objective 9A